

HEAVY VEHICLE LOCAL ROAD REPORT

Sydney Metro West – Western Tunnelling Package
Sydney Olympic Park

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B	02/11/2023	Revised following stakeholder comments
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D	05/12/2023	Revised following comments from Department of Planning

Document Authorisation

Action Type	Position	Name	Signature	Date Signed
Prepared by	Logistic Manager	Daniel Kelly		05/12/2023
Reviewed by	Project Manager	Jean-Francois Kielt		05/12/2023
<p>I hereby confirm this activity and all associated work, have been appropriately planned and the relevant resources are available to conduct the work in accordance with the agreed method.</p> <p>I hereby approve this activity to commence, as the stated controls applications are the most appropriate and are in accordance with the Risk Matrix.</p>				
Approved by	Project Manager	David Leaver		05/12/2023

NOTES:

Once all signatures have been obtained, the Document Author is responsible for ensuring the signed and approved hard and soft copies are uploaded on to the project share drive or passed to the Responsible Person for filing.

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Definitions/ Abbreviations

Acronym	Definition
BNS	Burwood North Station (not applicable to WTP works)
CPC	City of Parramatta Council
CEMP	Construction Environmental Management Plan
CJP	Customer Journey Planning (formerly SCO/ TC)
CLY	Clyde site
CMSF	Clyde Main Stabling Facility
CTMF	Construction Traffic Management Framework
CTMP	Construction Traffic Management Plan
CC	Cumberland Council
DMS	Delivery Management System
DPE	Department of Planning and Environment
EIS	Environmental Impact Statement
EPA	Environmental Protection Agency
EPL	Environment Protection License
FDS	Five Dock Station (not applicable to WTP works)
GLC	Gamuda and Laing O'Rourke Consortium
HVLR	Heavy Vehicle Local Road report
LTC	Local Traffic Committee (Council)
MCoA	Ministerial Conditions of Approval
NSMS	North Strathfield Metro Station (not applicable to WTP works)
OSOM	Over Size and Over Mass vehicles
PMS	Parramatta Metro Station
RMS	Roads and Maritime Service (now part of TfNSW)
ROL	Road Occupancy License
ROP	Road Opening Permit
SCO	Sydney Coordination Office (now known as CJP)
SMW	Sydney Metro West
SOP	Sydney Olympic Park
SOPA	Sydney Olympic Park Authority
SOPMS	Sydney Olympic Park Metro Station
SZA	Speed Zone Authorisation
TBM	Tunnel Boring Machine
TBS	The Bays Station (not applicable to WTP works)

Acronym	Definition
TC	Transport Coordination (formerly SCO now known as CJP)
TCG	Traffic Control Group
TCP	Traffic Control Plan (now known as TGS)
TfNSW	Transport for New South Wales
TGS	Traffic Guidance Scheme (formerly TCP)
TMC	Transport Management Centre
TTLG	Traffic and Transport Liaison Group
REMM	Revised Environmental Management Measure
WMS	Westmead Metro Station

1 EXECUTIVE SUMMARY

This Heavy Vehicle Local Road report (HVLR) has been developed to address the requirements of the Ministerial Conditions of Approval related to the Critical State Significant Infrastructure #10038 Stage 1 of the Sydney Metro West project.

The HVLR identifies the heavy vehicle routes into the sites not identified in the Environmental Impact Statement, the road classification and the suitability of the routes based on swept path analysis and adjacent land uses.

For the Sydney Olympic Park site, the routes proposed into site for the works are via Australian Ave, Sarah Durack Ave, Olympic Blvd and Herb Elliott Ave. Herb Elliott between Australia Ave and the site will only be used during events when notified by SOPA due to major event bussing.

The suitability of the routes has been assessed based on typical Heavy Vehicle sizes eg: 19m semi-trailers. The swept paths show that the vehicles operate satisfactorily to and from the site.

2 INTRODUCTION

Sydney Metro is Australia’s biggest public transport project, with the vision “to transform Sydney with a world-class metro.” In 2024, Sydney will have 31 metro stations and more than 66 kilometres of new metro rail, revolutionising the way Australia’s biggest city travels. By the end of the decade, the network will be expanded to include 46 stations and more than 113 kilometres of world-class metro for Sydney.

Sydney Metro West is a new 24-kilometre metro line with stations confirmed at Westmead, Parramatta, Sydney Olympic Park, North Strathfield, Burwood North, Five Dock, The Bays, Pyrmont and Hunter Street in the Sydney CBD.

On completion in 2030, the Sydney Metro West project will support a growing city and deliver world-class metro services to more communities. This new underground railway will connect Greater Parramatta and the Sydney CBD.

This once-in-a-century infrastructure investment will transform Sydney for generations to come, doubling rail capacity between the two CBDs, linking new communities to rail services, and supporting employment growth and housing supply. The Sydney Metro West project is expected to create about 10,000 direct and 70,000 indirect jobs during construction.

The new 24-kilometre Sydney Metro West tunnel and excavation works for nine new stations will be delivered in three contracts—the Western Tunnelling Package (WTP), the Central Tunnelling Package (CTP) and the Eastern Tunnelling Package (ETP).

The Gamuda Australia and Laing O’Rourke Consortium (GLC) will deliver the Sydney Metro West (SMW) Western Tunnelling Package (WTP), which includes:

- Westmead Station box excavation, including temporary support, stub tunnels, partially mined station cavern and crossover cavern including permanent lining and support
- Parramatta Station, including excavation of station box and associated support
- Clyde Maintenance and Stabling Facility (MSF), including permanent dive structure, portal, spur running tunnels, spur tunnel junction cavern, bulk earthworks, civil structures, utilities corridor, road crossing and creek diversion
- Rosehill Services Facility, including shaft excavation, permanent lining and lateral support
- A precast segment manufacturing facility at Eastern Creek
- Demolition and site clearance works

Tunnelling between Sydney Olympic Park (SOP) and Westmead. Tunnelling will be undertaken by placing the tunnel boring machines (TBMs) at the Rosehill Services Facility box and retrieved out at the SOP Station Box and then placed back at the Rosehill Services Facility and retrieved at the Westmead Station Box. No surface works are proposed at SOP except for the retrieval of the TBM.

2.1 Purpose

This heavy vehicle Local Road (HVLRL) report details the heavy vehicle routes as noted in the Environmental Impact Statement for the project and the proposed routes to be used for the Sydney Olympic Park site access/ egress.

This report is a sub-plan to the site-specific Construction Traffic Management Plan for the Sydney Olympic Park site and has been prepared in accordance with Gamuda Australia and Laing O'Rourke Consortium (GLC) legal, planning and contractual requirements and environmental management system (EMS) including compliance to the Ministerial Conditions of Approval (MCoA), Revised Environmental Management Measures (REMM) and the Construction Traffic Management Framework

This report enables the project to manage potential construction traffic impacts systematically and is applicable to the Sydney Olympic Park Site and all project activities.

2.2 Planning approval

Sydney Metro West – Westmead to The Bays Concept and Stage 1 was subject to environmental impact assessment under the NSW Environmental Planning and Assessment Act, 1979 (EP&A Act). It was declared a Critical Stage Significant Infrastructure (CSSI) by the Minister for Planning and Public Spaces.

An Environmental Impact Statement (EIS) was prepared under Division 5.2 of the EP&A Act and in accordance with Part 3 of Schedule 2 of the Environmental Planning and Assessment Regulation, 2000. Following exhibition of the EIS, an Amendment Report and Submissions Report was also prepared, after which the Minister carried out an assessment and made a determination.

The planning approval (Infrastructure Approval SSI 100038) and related environmental assessment documents are located at [Sydney Metro West - Concept and Stage 1 \(major civil construction between Westmead and The Bays\) | Planning Portal - Department of Planning and Environment \(nsw.gov.au\)](https://pp.planningportal.nsw.gov.au/major-projects/projects/sydney-metro-west-concept-and-stage-1-major-civil-construction-between-westmead-and-the-bays)

Website: <https://pp.planningportal.nsw.gov.au/major-projects/projects/sydney-metro-west-concept-and-stage-1-major-civil-construction-between-westmead-and-bays>

3 COMPLIANCE

3.1 Ministerial Conditions of Approval

The Ministerial Conditions of Approval are listed below in Table 1.

Table 1: Ministerial Conditions of Approval

MCoA	Condition requirement	Document reference
A47	All heavy vehicles used for spoil haulage must be clearly marked on the sides and rear with the project name and application numbers to enable immediate identification by a person viewing the heavy vehicle standing 20m away	Table 10
D86	Local roads proposed to be used by Heavy Vehicles to directly access construction sites that are not identified in the documents listed in Condition A1 of this schedule must be approved by the Planning Secretary and be included in the CTMPs	This report
D87	All requests to the Planning Secretary for approval to use local roads under Condition D86 above must the following a) A swept path analysis	Appendix A
D87	All requests to the Planning Secretary for approval to use local roads under Condition D86 above must the following b) Demonstration that the use of local roads by Heavy vehicles for the CSSI will not compromise the safety of pedestrians and cyclists of the safety of two way traffic flow on two way roadways	This report
D87	All requests to the Planning Secretary for approval to use local roads under Condition D86 above must the following c) Details as to the date of completion of the road dilapidation surveys for the subject local road and	Appendix D
D87	All requests to the Planning Secretary for approval to use local roads under Condition D86 above must the following d) Measures that will be implemented to avoid where practicable the use of local roads past schools, aged care facilities and child care facilities during their peak operation times and	This report
D87	All requests to the Planning Secretary for approval to use local roads under Condition D86 above must the following e) Written advice from an appropriately qualified professional on the suitability of the proposed Heavy Vehicle route which takes into consideration items a) to d) of this condition	Appendix B
D88	Before any local road is used by a Heavy Vehicle for the purposes of construction of Stage 1 of the CSSI, a Road Dilapidation Report must be prepared for the road. A copy of the Road Dilapidation Report must be provided to the Relevant Road Authority(s) within three (3) weeks of completion of the survey and at no later than one (1) month before the road being used by Heavy Vehicles associated with the construction of Stage 1 of the CSSI	Section 7.1 and Appendix D Dilap was completed on 11 th October 2023 and

MCoA	Condition requirement	Document reference
		issued to SOPA on 8 th November 2023.
D89	If damage to roads occurs because of the construction of Stage 1 of the CSSI, the Proponent must either (at the Relevant Road Authority's discretion) a) Compensate the Relevant Road Authority for the damage so caused or b) Rectify the damage to restore the road to at least the condition it was in pre-work as identified in the Road Dilapidation Report	Section 7.1
D90	Vehicles associated with the project workforce (including light vehicles and Heavy Vehicles must be managed to: a) Minimise parking on public roads	Section 6.1.2
	Heavy Vehicles must be managed to: b) Minimise idling and queuing on state and regional roads	Section 7
	Heavy Vehicles must be managed to: c) Not carry out marshalling of construction vehicles near sensitive land user(s)	Section 7
	Heavy Vehicles must be managed to: d) Not block or disrupt access across pedestrian or shared user paths at any time unless alternate access is provided and	Section 6.1.3 & Section 7
	Heavy Vehicles must be managed to" e) Ensure spoil haulage vehicles adhere to the nominated haulage routes identified in the CTMPs	Section 7

3.2 Revised Environmental Management Measures

The Revised Environmental Management Measures are listed below in Table 2

Table 2: Revised Environmental Management Measures

REMM#	Condition requirement	Site(s)	Document Reference
TT6	All trucks would enter and exit construction sites in a forward direction, where feasible and reasonable	All	Section 6.1.1
TT7	Construction site traffic would be managed to minimise movements during peak periods	All	Section 6.1.1
TT8	Construction site traffic immediately around construction sites would be managed to minimise vehicle movements through school zones during pick up and drop off times	WMS PMS BNS FDS	Not applicable to SOP site

4 LEGAL AND OTHER REQUIREMENTS

4.1 Relevant Legislation

Identified regulatory requirements are:

- An approved and valid Road Occupancy Licence (ROL)
- An approved relevant Speed Zone Authorisation (SZA)
- Australian Road Rules form the basis for state and territory road rules
- *Roads Act, 1993* (NSW) sets out rights along a public road, establishes procedures for a public road and provides the classifications of roads
- *Heavy Vehicle National Act 2013 and Regulation, 2013* (NSW)
- *Heavy Vehicle (Adoption of National Law) Act, 2013* (NSW)
- *Dangerous Goods (Road and Rail Transport) Act, 2008*
- Road and Rail Transport (Dangerous Goods) (Road) Regulation, 1998
- Australian Code for the Transport of Dangerous Goods by Road and Rail (National Transport Commission, 2008)
- Dangerous Goods (Road and Rail Transport) Regulation, 2014
- Australia Code for the Transport of Dangerous Goods by Road and Rail Edition 7.7 (National Transport Commission, 2020)
- *Environmental Planning and Assessment Act, 1979* – under which the project approval was granted including the Environmental Impact Statement and Construction Traffic Management Framework

4.2 References and guidelines

The relevant standards, codes and guidelines are noted below:

- AustRoads Cycling Aspects of AustRoads Guides, 2017
- AustRoads Guide to Traffic Management, 2020 – Parts 1-13
- AustRoads Guide to Road Design, 2013 to 2021-Parts 1-7
- AustRoads Guide to Road Safety, 2019 to 2021 – Parts 1-7
- Roads and Traffic Authority, NSW Guide to Traffic Generating Developments, 2002 and further updates as provided
- Roads and Traffic Authority, NSW Bicycle Guidelines, version 1.2, 2005
- Roads and Maritime QA Specification G10, Traffic Management, 2020
- Roads and Maritime NSW Speed Zoning Guidelines, 2011
- TfNSW Traffic Control at Worksites Manual, version 6.1, 2022 and
- TfNSW NSW Substantiable Design Guidelines, version 4, 2017

5 THE EXISTING ENVIRONMENT

5.1 Locality and land use

The site is located in Sydney Olympic park south of the Sydney Trains Station and is bounded by Olympic Boulevard to the west, Figtree Drive to the south, Australia Avenue to the east and Herb Elliott Avenue to the north as shown on Figure 1 below.



Figure 1: Site locality

There is currently no schools, aged care facilities or existing childcare facilities directly on any of the proposed haul routes. GOAL College Secondary School is located off Sarah Durack Avenue and is 150m away from our haul route and PCYX OOSY- out of school hours care is located on Figtree drive approximately 190m away from our intended haul route – refer to figure 2 below.

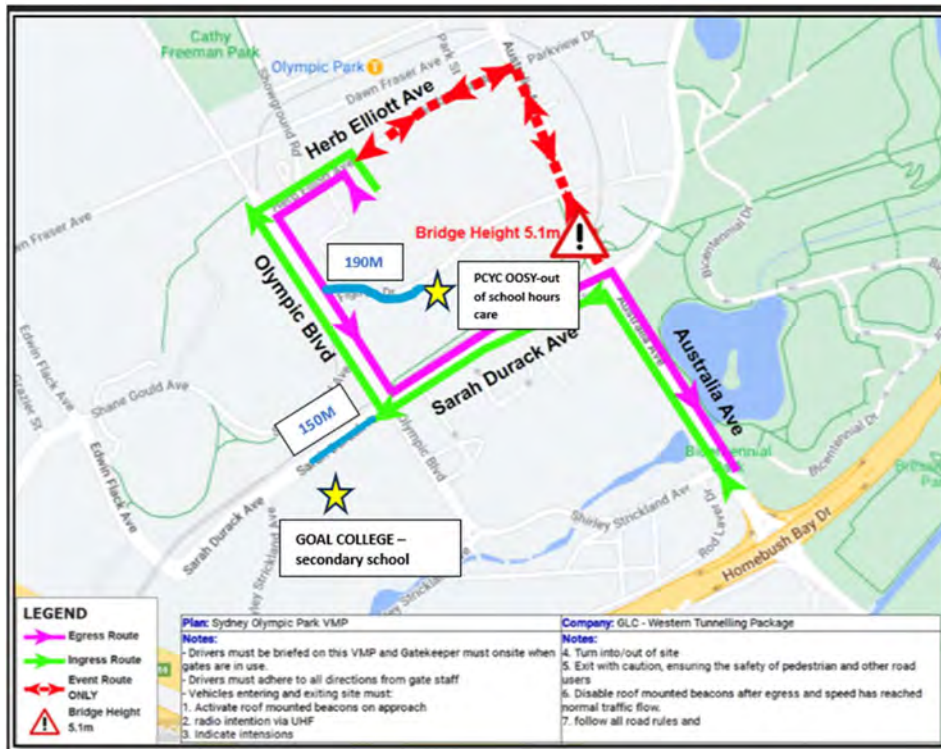


Figure 2: Location of Vulnerable road users

Goal College – Secondary school college. **(150 metres away from TBM route)**

Address: Olympic Blvd, Sydney Olympic Park NSW 2127

Opening hours: Mon-Fri 8:30am-2:30pm

PCYC OOSH – Out of school hours care **(190 metres away from TBM route)**

Address: 2/6B Figtree Dr, Sydney Olympic Park NSW 2127

Opening hours: Mon-Fri 8:00am-6:00pm

There is no aged care facilities on or near our haul routes.

Footpaths exist on both sides of the street. Signalised pedestrian crossings are provided at the intersection of Sarah Durack Avenue / Sydney Olympic Blvd. Our heavy haulage vehicles will not be using Figtree drive and pedestrians wishing to cross Olympic Blvd would be able to using the existing pedestrian crossing.

Access and egress to the M4 Motorway carriageway is via Homebush Bay Dr at Australia Ave. All of these roads are classified as state roads as shown on figure 2 below.

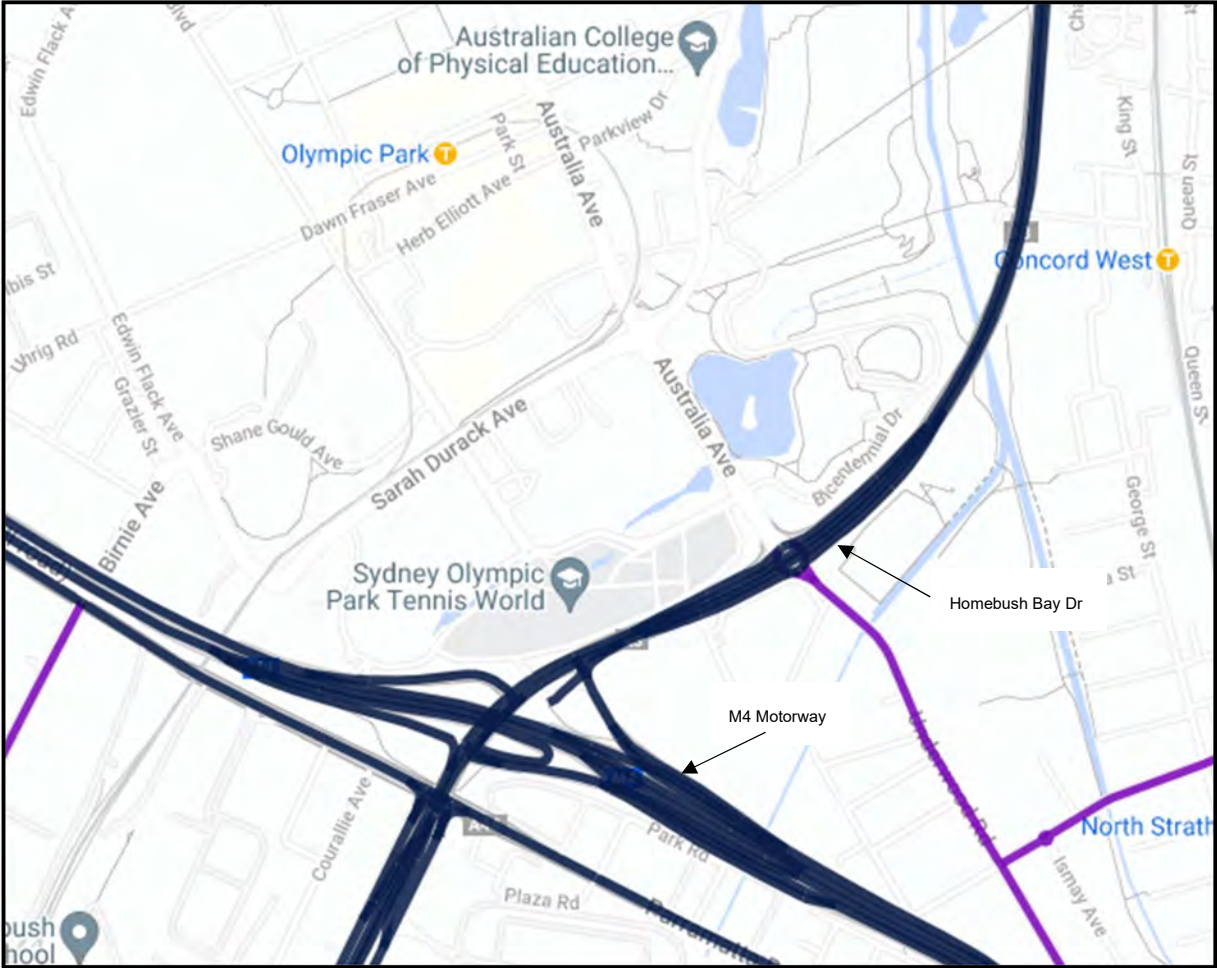


Figure 3: NSW Road Classification Map (source: [TfNSW Road Network Classification Map](#))

5.2 Australia Avenue

Australia Avenue is a major road through the Sydney Olympic Park precinct. Australia Avenue is a two-way road with two traffic lanes in each direction. Parking is prohibited along both sides of the road. The posted speed restriction on Australia Avenue is 60km/h.

5.3 Herb Elliott Avenue

Herb Elliott Avenue is a SOPA road primarily serving access to commercial office buildings. Herb Elliott Avenue is a two-lane, two-way road with a road carriageway width of approximately 13m. Ticketed kerbside parking is available along both sides of the road. The posted speed restriction on Herb Elliott Avenue is 40km/h.

5.4 Figtree Drive

Figtree Drive is a minor road within the Sydney Olympic Park precinct serving access to existing commercial buildings and recreational facilities. Figtree Drive has a road carriageway width of approximately 7m to accommodate the eastbound and westbound travel lanes. There are some sections of indented parallel parking bays for restricted parking (2P) along both sides of the road. The posted speed restriction on Figtree Drive is 40km/h.

5.5 Olympic Boulevard

Olympic Boulevard is a north-south road between Kevin Coombs Avenue and Shirley Strickland Avenue. Olympic Boulevard is configured with two lanes in each direction separated by a 4m wide central median between Dawn Fraser Avenue and Shirley Strickland Avenue. From north of Dawn Fraser Avenue to south of Shirley Strickland Avenue, the road width is approximately 18m. There is a mixture of timed parking and bus bays along the route. The parking is reserved for the public and should no time be used to stable heavy vehicles.

5.6 Sarah Durack Avenue

Sarah Durack Avenue is one of the main roads that bound the Sydney Olympic Park precinct. Sarah Durack Avenue is configured with two traffic lanes in each direction separated by a 4.5m wide central median in the east-west alignment. Parking is prohibited along both sides of the road. In addition, there are on-road cycle lanes along the north and south sides of the road.

6 SITE OPERATIONS

The Sydney Olympic Park site has been established by the Central Tunnel Package contractor and a portion of the site will be handed over to GLC. GLC will establish our site in the areas highlighted in green on the plan shown in 3 below. The driveway from Herb Elliott Ave will be a shared entry and exit driveway between GLC and CTP contractors. Site access and egress on Herb Elliott Ave will be as per the EIS. The gate will have 'No Entry' - 'Construction Vehicles Excepted' and the gate number signage installed.

All works external to the site will be completed under Traffic Guidance Scheme's and Road Occupancy approvals through TfNSW and SOPA.

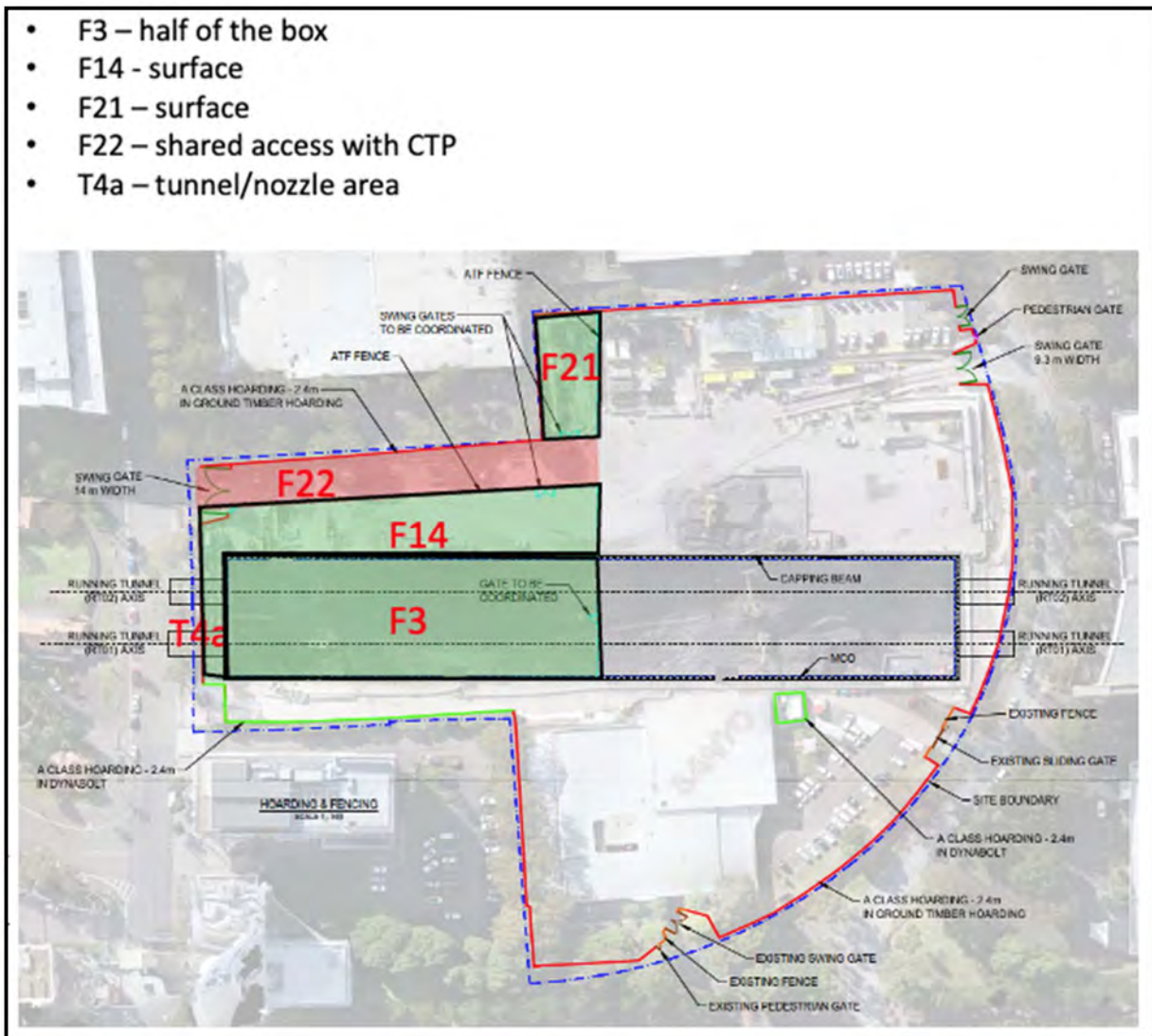


Figure 4: Sydney Olympic Park Site Overview

SOP site both during GLC Site Establishment and Site Operations Heavy vehicles will follow the proposed heavy vehicle route and will entry site from Herb Elliott Ave eastbound right turn and exit via left turn on to Herb Elliott Ave onto Herb Elliott Ave to Olympic Blvd.

The Sydney Olympic Park site has been established by the Central Tunnel Package contractor and a portion of the site will be handed over to GLC. GLC will establish our site in the areas highlighted in green on the plan shown in 5 below. The driveway from Herb Elliott Ave will be a shared entry and exit driveway between GLC and CTP contractors. Site access and egress on Herb Elliott Ave will be as per the EIS. The gate will have 'No Entry' - 'Construction Vehicles Excepted' and the gate number signage installed.

All works external to the site will be completed under Traffic Guidance Scheme's and Road Occupancy approvals through TfNSW and SOPA.

SOP site both during GLC Site Establishment and Site Operations Heavy vehicles will follow the proposed heavy vehicle route and will entry site from Herb Elliott Ave eastbound right turn and exit via left turn on to Herb Elliott Ave onto Herb Elliott Ave to Olympic Blvd.

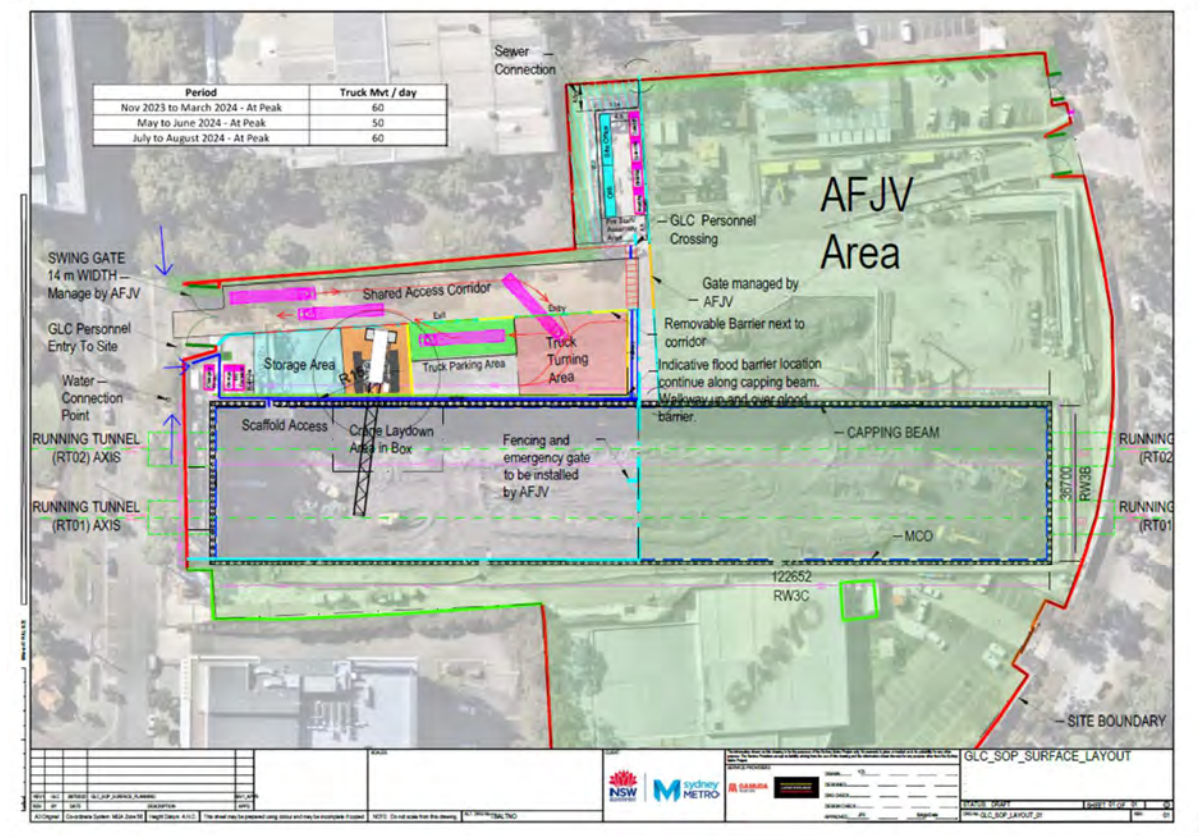


Figure 5: Sydney Olympic Park Internal Site Overview

The figure above demonstrates how the site will be laid out to facilitate heavy vehicle movements within the GLC project boundary.

All work vehicles will enter and exit the construction sites in a forward direction, where reasonable and feasible. Where this is not possible, appropriate management measures will be put in place such as traffic control.

6.1.1 Impact on traffic flow

GLC will ensure that impacts to traffic flow are minimised by:

- Centralised logistics team and monitoring area to identify and regulate truck operations
- Managing truck movements along discrete routes to minimise cumulative haulage impacts
- Scheduling where feasible, to avoid operating school zones and high pedestrian activity areas

6.1.2 Impact on parking

All heavy vehicles associated with the works will be catered for on site. Light vehicles will be managed as per details set out within the Car Parking and Access Strategy (CPAS) for Sydney Olympic Park.

6.1.3 Impact on active transport users

Sydney Olympic Park is connected with footpaths and shared paths provided along both sides of all roads. There are on-road cycleways available on Australia Avenue, Kevin Coombs Avenue, Edwin Flack Avenue, Sarah Durack Avenue, Olympic Boulevard, Marjorie Jackson Parkway and Bennelong Parkway. The recreational and parkland areas within Sydney Olympic Park precinct have shared paths available throughout the area. Dates for the retrieval of the TBM will take into account any events within SOPA and assess the impact and adjust dates as required.

Truck aware decals have been implemented by the current Sydney Metro contractor at the Sydney Olympic Park site. This signage and decal are shown below on figure 6 below.



Figure 6: Shared path warning sign and truck decals

GLC will attend regular coordination meetings with Sydney Olympic Park and will provide advance notification of Oversize Overmass nighttime heavy vehicle movements to enable community notifications and to assist with coordinating works outside of event times where pedestrian and vehicle movements are increased.

6.1.4 Managing cumulative impacts

No works have been identified outside the Sydney Olympic Park work site. To ensure that GLC are aware of other projects and/ or impacts, GLC will attend the Traffic Control Group (TCG) and Traffic and Transport Liaison Group (TTLG) forums. Work inside the site and access using haulage routes will be coordinated with Sydney Olympic Park and AFJV to mitigate impact. GLC will attend a monthly meeting with Sydney Olympic Park committee to discuss any upcoming events and the impact GLC traffic operations may have on them. AFJV will still be present at the Metro Sydney Olympic Park Site and will retain Principal Contractor of a part of the site where they have ongoing construction operations.

6.1.4.1 Traffic and Transport Liaison Group (TTLG)

This forum is provided to ensure that the stakeholders who may be affected by the works are informed of the proposed works and the traffic impacts associated with those works. Attendance at these forums typically includes several government agencies at both the state and local level, emergency services, public and private transport operators, pedestrians and cyclists advocacy groups and other stakeholders as nominated by TfNSW. This forum typically meets monthly.

6.1.4.2 Traffic Control Group (TCG)

The TCG is a forum to discuss the proposed traffic management measures during the stages of the works including the impacts on the road and transport network and proposed mitigation measures, any feedback received on the traffic documentation and updates on the program of works. This group will meet as agreed between the members. The TCG members will vary depending on the location of the works. Proposed members include:

- Transport for New South Wales including Sydney Metro; Transport Management Centre: Customer Journey Planning (previously SCO); Greater Sydney Region (previously RMS)
- Local council representatives
- Other contractors required by TfNSW
- Centre of Road and Maritime Safety
- Infrastructure NSW.

7 FLEET MANAGEMENT

Trucks to be used on the project will be compliant with NSW legislation, Sydney Metro’s Principal Contractor Health and Safety Standard, relevant Australian Design Rules and vehicle standards and the Heavy Vehicle National Legislation. All heavy vehicle operations will be conducted in accordance with GLC’s Chain of Responsibility (CoR) Management Plan including compliance with nominated haulage routes.

A combination of truck types will be used during the works, with trucks being truck and dog, semi-trailers, 12.5m single unit trucks and low loaders. Table 10-4 of Chapter 10 Traffic and Transport notes the vehicle types to be used at the Sydney Olympic Park site, as noted below.

Table 10-4: Construction vehicle types

Construction site	Truck type ¹
Westmead metro station	Rigid truck and/or truck and dog
Parramatta metro station	Rigid truck and/or truck and dog
Clyde stabling and maintenance facility	Rigid truck and/or truck and dog
Silverwater services facility	Rigid truck and/or truck and dog
Sydney Olympic Park metro station	Rigid truck and/or truck and dog
North Strathfield metro station	Rigid truck and/or truck and dog
Burwood North Station	Rigid truck and/or truck and dog Medium rigid truck
Five Dock Station	Rigid truck and/or truck and dog
The Bays Station	Rigid truck and/or truck and dog

All vehicles will enter and exit the site in a forward direction.

Construction traffic will be managed to minimise movements during peak periods and through school zones during drop off and pick up times, in particular at the Westmead and Parramatta sites and this will be achieved through scheduling of vehicles and staggered start and finish times. GLC will provide sufficient onsite parking for heavy vehicles associated with the works. This will ensure that vehicles are not idling or queuing on state, regional and local roads. In the event that vehicles are unable to be accommodated on our other sites, vehicles will be directed to the Clyde site as an extended marshalling facility. Given the amount of space available at the Clyde site there is no requirement for any further marshalling facilities.

7.1 Road dilapidation report

Road dilapidation reports has been provided for the local roads used by construction vehicles. These reports will be undertaken prior to the use of these roads. A copy of the report has been provided to the relevant road authority on 27th October 2023 and issued to Sydney Olympic Park Committee on the 8th November 2023. The requirement is to submit this plan within three weeks of completing the survey and no later than one (1) month before the road is used.

If damage to roads occurs as a result of heavy vehicle use associated with the construction works, GLC, will, at the relevant road authority’s discretion:

- Compensate the relevant road authority for the damage so caused or
- Rectify the damage to restore the road to at least the condition it was in pre-work as identified in the road dilapidation report

A copy of the Road Dilapidation Report transmittal to the Sydney Olympic Park Authority has been provided separately.

Table 3 below provides a summary of the sections of local road GLC is proposing to use.

Local Roads	Length TBM occupy the road (in metres)
Herb Elliott Avenue	180m
Olympic Boulevard	390m
Sarah Durack Avenue	420m
Australia Avenue	540m
Homebush Bay Drive	790m

Table 3: Summary of roads GLC is proposing to use

7.2 Drivers and operators

Operator selection will be based on safety performance criteria. Operators and drivers will be required to have general construction industry induction cards and will be required to attend ongoing general project and site specific inductions.

All operators will be comprehensively trained with regard to community expectations and impacts from heavy vehicle movements through site inductions and attendance at the Sydney Metro Industry Curriculum (SMIT) – Safe Heavy Vehicle Introduction Skills which provides drivers with the knowledge, skills, motivation and confidence to drive heavy vehicles safely and professionally in an urban built up road environments, whilst undertaking a transport task required on the project. The training course focuses on low risk driver behaviours, shared the road safely with vulnerable road users and reinforces heavy vehicle driver knowledge and skill. The project and site inductions will have a particular focus on operator behaviour. The driver induction process will include safety awareness in relation to all road users, particularly pedestrians and cyclists.

7.3 Proposed vehicle movements

The EIS for the Sydney Metro West Stage 1 project, noted for light vehicles that the site operations phase of the works would have distinct peak travel periods, typically prior and post the standard construction hours and that light vehicle numbers would be fairly constant over the work day, refer to figure 7. GLC works at SOP site are minimal (minor mobilisation, headwall construction and TBM retrieval) and would only generate minimal numbers of heavy and light vehicles to the site. GLC have confirmed with AFJV that they are currently using EIS Sydney Metro West Stage 2 vehicle movement numbers & have two approved haulage routes. AFJV Haulage routes for Option 1 & Option 2 are provided below in figure 7 & 8.

Figure 7: AFJV Option 1

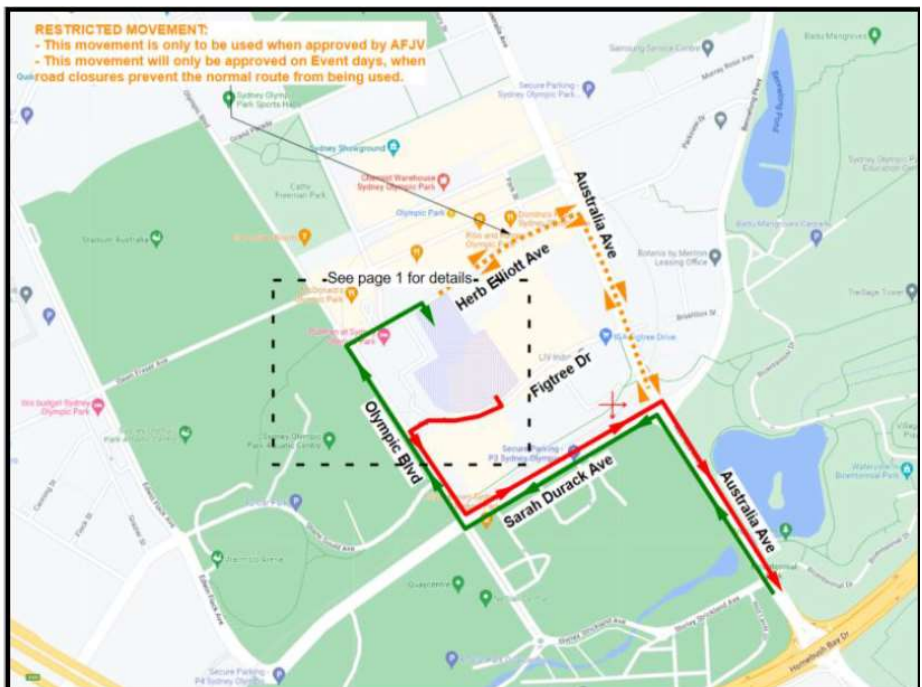
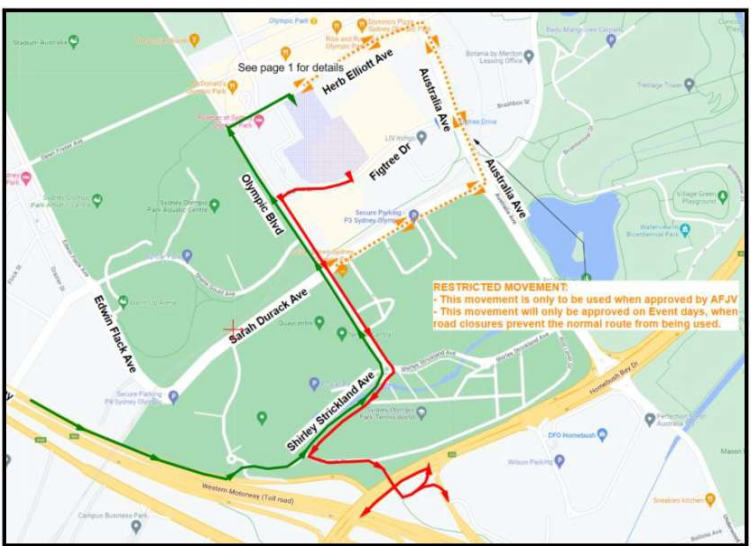


Figure 8: AFJV Option 2



AFJV are using Route Option 1 (Figure 7) as their primary route. Route Option 2 (Figure 8) is an AFJV secondary route, used only during peak times to relieve pressure on the surrounding network. By the end of the year, AFJV’s intention is to stay on ‘EIS stage 2’ and not use route option 2 (Figure 8) so their vehicle movements will reduce to 8 and they will only be using route 1 (Figure 7).

Table 4: AFJV Site Operations heavy movements (numbers) per hour (November 2023- December 2023)

AFJV Numbers 2023								
EIS Stage	Route Option	07:00	08:00	09:00	16:00	17:00	Total	Total both routes
		– 08:00	– 09:00	– 10:00	– 17:00	– 18:00		
2	1	8	8	8	8	8	156	306
	2	30	30	30	30	30	150	

Table 5: AFJV Site Operations heavy movements (numbers) per hour (January 2024 onwards)

AFJV Numbers 2024							
EIS Stage	Route Option	07:00	08:00	09:00	16:00	17:00	Total
		– 08:00	– 09:00	– 10:00	– 17:00	– 18:00	
2	1	8	8	8	8	8	156

Table 6: GLC Site Operations heavy movements (numbers) per hour

GLC Numbers							
07:00	08:00	09:00	10:00	16:00	17:00	18:00	Total
– 08:00	– 09:00	– 10:00	– 16:00	– 17:00	– 18:00	– 06:00	
8	8	8	8	8	8	8	156

Table 7: GLC and AFJV Combined heavy movements (numbers) per hour (January 2024 – onwards)

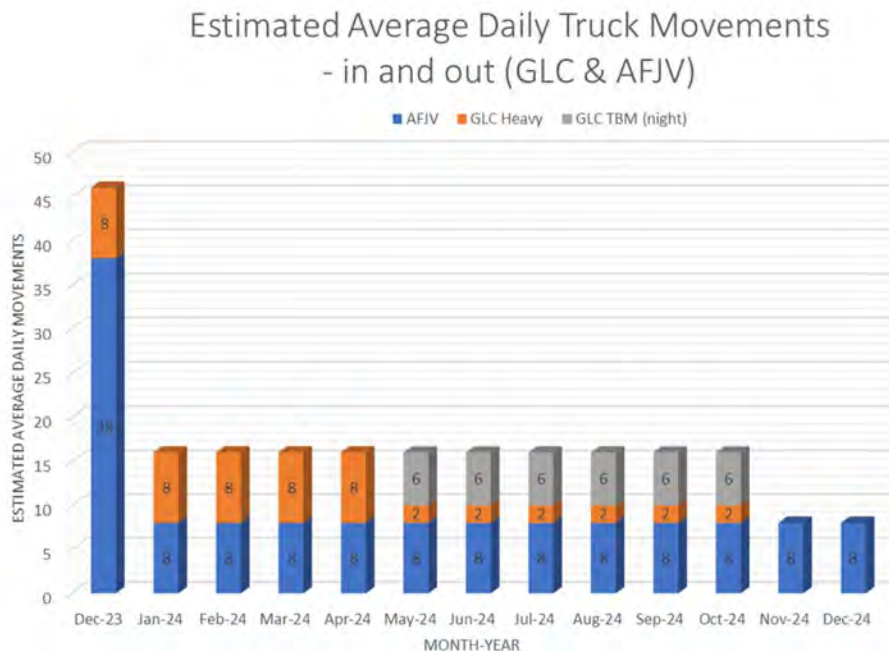
GLC			AFJV – January 2023 onwards			EIS Numbers			GLC & AFJV Combined
Peak Times	Outside of peak	Total	Peak Times	Outside of peak	Total	Peak Times	Outside of peak	Total	
8	8	156	8	8	156	8	14	306	312

Given the limited construction scope (headwall construction only) GLC has at the Sydney Olympic Park Site the movements per hour will be for concrete operations only, where at peak (Feb 2024 – April 2024) we will require between 6-8 concrete agitators per hour, but only on two to three days a week; this may include a Saturday. so the day-to-day numbers will be significantly below the 6 to 8 number. Figure 9 below shows the average daily truck movements for GLC and AFJV combined, again noting that on some days GLC will not be running concrete agitators

Note: that while Figure 9 shows increased combined movements in December 2023 this will only be until Dec 22nd when both sites will shut down for the Christmas period. Work begins again on

3rd January and it will begin slowly not reaching the number shown in Figure 9 and Table 7 until at least the 3rd week of January 2024.

Figure 9: Estimated Average Daily Truck Movements.



For heavy vehicle movements, the EIS predicted movements were reduced during the AM and PM peak periods and evenly spread over the course of the rest of the work day, refer to Figure 10 below.

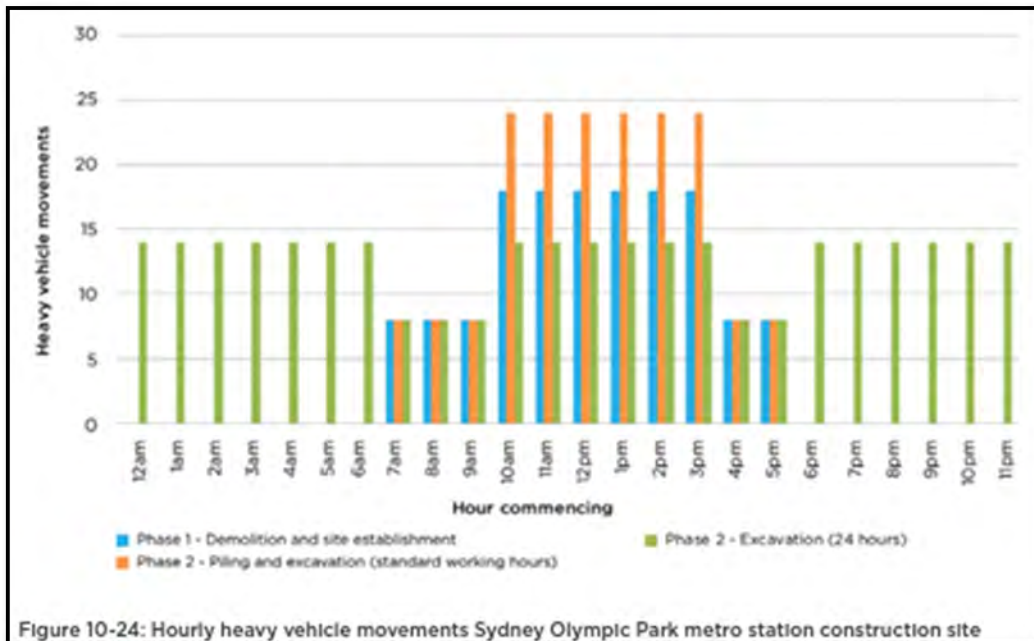


Figure 10: EIS Hourly Heavy Vehicle Movements (source: EIS Chapter 10 page 10-13)

Table 8: GLC Site Operations light vehicle movements (numbers) per hour

Time	GLC Light	AFJV Light	EIS	Combined Numbers
6AM to 7AM	15	40	46	61
7AM to 8PM	15	40	46	61
8AM to 9AM	10	40	46	56
9AM to 2PM	12	40	46	58
2PM to 3PM	12	40	46	58
3PM to 4PM	12	40	46	58
4PM to 5PM	15	40	46	61
5PM to 6PM	15	40	46	61
6PM to 9PM	10	40	46	56
9PM to 11PM	8	12	46	54
11PM to 6AM	4	2	46	50

Note: GLC numbers are significantly under the AFJV numbers.

GLC is estimated to only require 25 No. light vehicles at anyone time. The maximum movements per hour are reflected above.

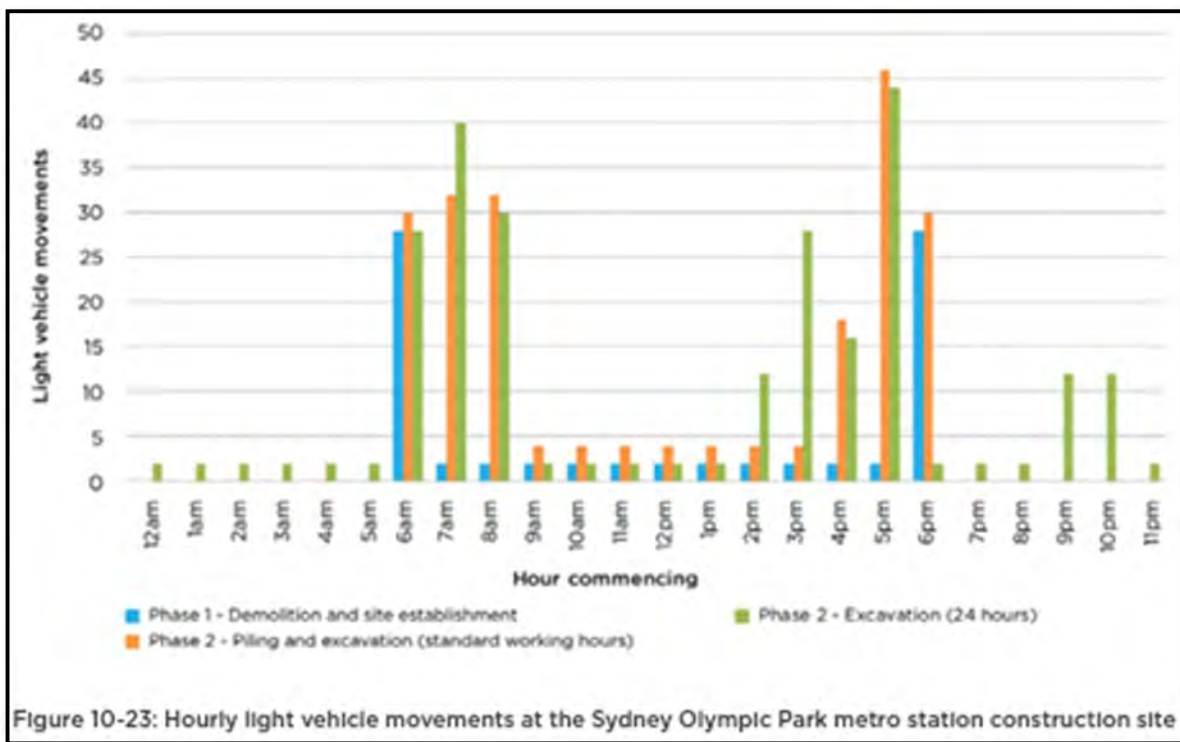


Figure 11: EIS Light Vehicle Movements

7.4 EIS routes for Heavy Vehicles

The EIS nominated the heavy vehicle haulage routes into and out of the site as shown on Figure 12 below.

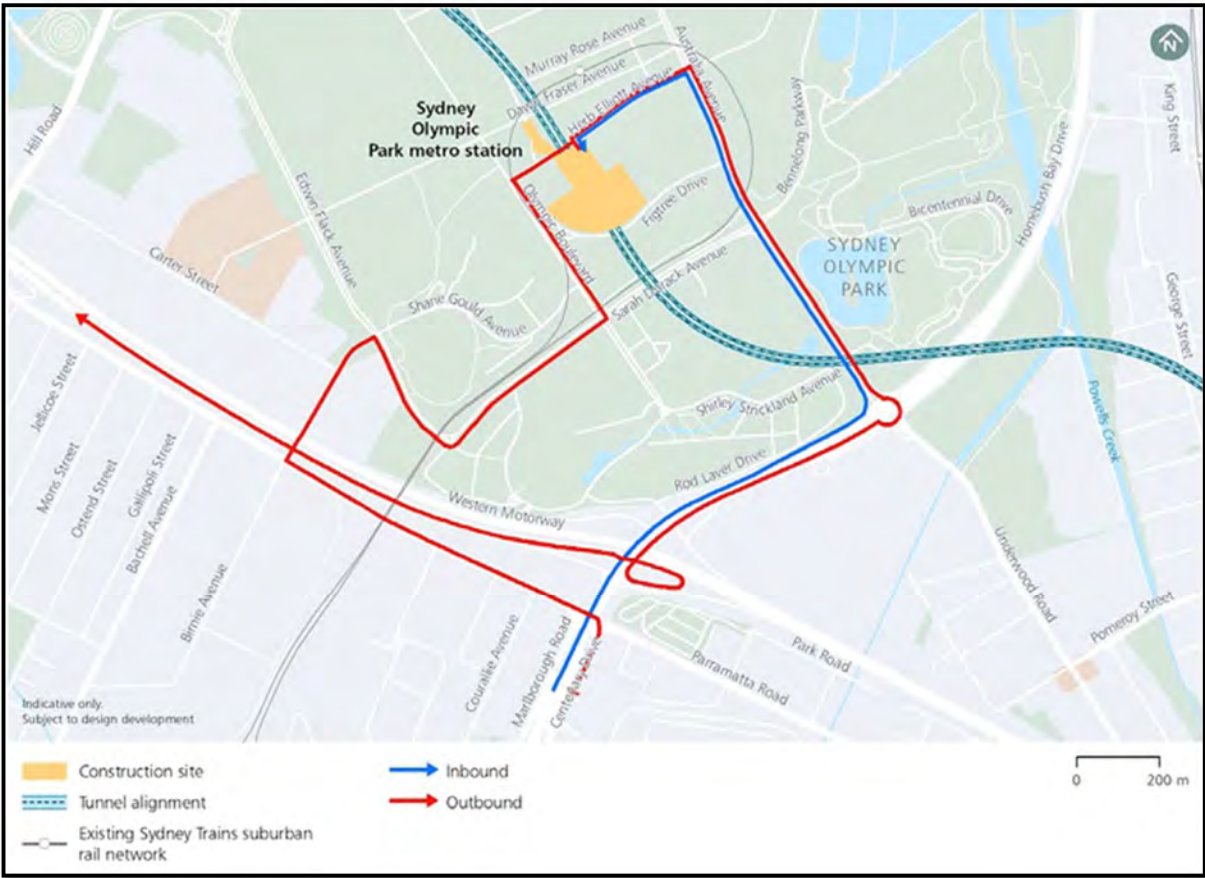


Figure 12: EIS nominated heavy vehicle routes

7.5 Proposed heavy vehicle routes

There are a number of local roads within the Sydney Olympic Park area that are required to access/ egress the construction site. The EIS nominates Australia Ave and Herb Elliott Ave to access and egress the site which is not the current approved CTP route. GLC propose to utilise, Australia Ave, Sarah Durack Ave, Olympic Blvd and Herb Elliott Ave as an entry and exit point into the area. Herb Elliott Ave between the site and Australia Ave will only be used on Special Event days when Olympic Blvd is closed for major event bussing operations during events. These roads are detailed in table 9 below.

Table 9: Roads to be used by Heavy Vehicles

Road name	Between	Between	Classification	Two way traffic flow	Parking	Speed limit
Australia Ave	Homebush Bay Dr	Herb Elliott Ave	Local	Yes	No	60km/hr
Herb Elliott Ave	Australia Ave	Olympic Blvd	Local	Yes	Yes	40km/hr
Olympic Blvd	Herb Elliott Ave	Sarah Durack Ave	Local	Yes	Yes	40km/hr
Sarah Durack Ave	Australia Ave	Olympic Blvd	Local	Yes	No	60km/hr

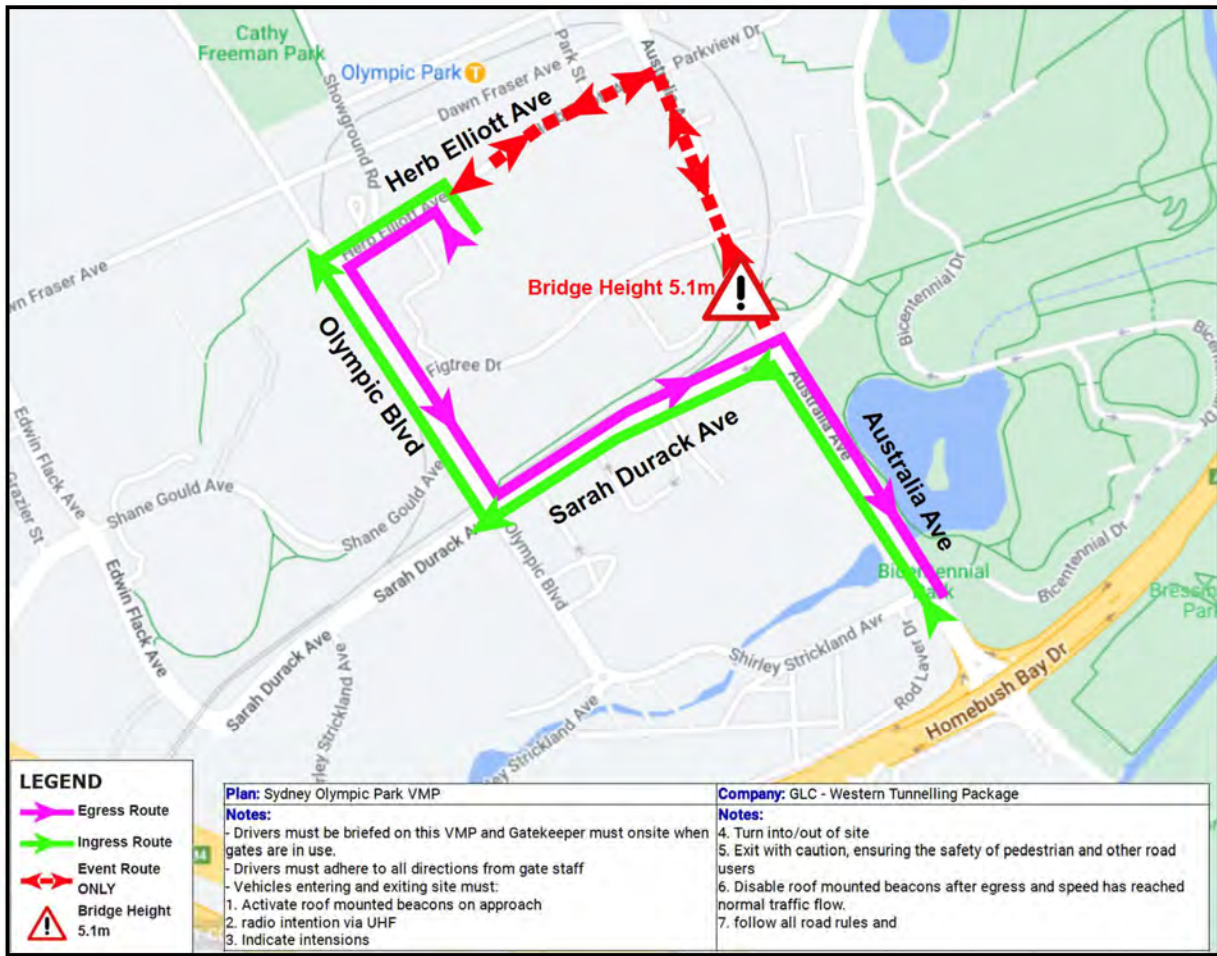


Figure 13: Proposed heavy vehicle routes & local roads not included in the EIS

7.6 Fleet safety

GLC is committed to safety for all aspects of the project with road safety being paramount to the success of the project. To demonstrate this commitment the requirements listed in Table 1010.

Table 10: Heavy vehicle requirements

Requirement(s)	Purpose	Managed by
Ensure all heavy vehicles are registered and comply with the Australian Design Rules	Ensure compliance with legislative requirements	Checking prior to attendance at site through subcontractor engagement All vehicles will be registered on Plant Assessor

Requirement(s)	Purpose	Managed by
Blind spot elimination or minimise front, side and rear blind spots, including <ul style="list-style-type: none"> • Class V and VI mirrors as per ADR14.02 where blind spots cannot be permanently eliminated • The prohibition of accessories that restrict the forward field of vehicles including opaque or chrome bug deflectors 	Ensure compliance with SWTC and increase visibility of active transport users	Checking prior to attendance at site through subcontractor engagement All vehicles will be registered on Plant Assessor
Side underrun protection fitted to both sides of the vehicle: <ul style="list-style-type: none"> • Between the front and rear axle of all rigid (SU) trucks and • Between the front axle/ landing legs and rear axle of trailers forming part of a combination 	Improved protection for active transport users	Checking prior to attendance at site through subcontractor engagement All vehicles will be registered on Plant Assessor
Signage placed on heavy vehicles including: <ul style="list-style-type: none"> • Rear warning signs alerting other roads users to the dangers of overtaking and • Front nearside signs warning pedestrians about walking close to the front of a moving or stationary heavy vehicle 	Increasing road safety awareness for all users	Checking prior to attendance at site through subcontractor engagement All vehicles will be registered on Plant Assessor
Full body line and contour conspicuity markings and reflective markings fitted to the drawbar of all trailers	Increasing visibility of heavy vehicles	Checking prior to attendance at site through subcontractor engagement All vehicles will be registered on Plant Assessor
Heavy vehicle drivers to complete the Sydney Metro	Training and induction to address safety of	Training and induction process

Requirement(s)	Purpose	Managed by
Safe Heavy Vehicle Driver Induction program or similar	pedestrians/ cyclists along street frontages	All heavy vehicle operators will be registered on Plant Assessor
All heavy vehicles used for spoil haulage must be clearly marked on the sides and rear with the project name and application number to enable immediate identification by a person viewing the heavy vehicle standing 20m away	Compliance with MCoA	Checking prior to attendance at site through subcontractor engagement All vehicles will be registered on Plant Assessor

7.7 Permits / Over dimensional vehicles

Permit issue for vehicles greater than 4.5 tonnes is through the National Heavy Vehicle Regulator (NHVR). This applies to special purpose vehicles (SPV) such as mobile cranes and other over size/over mass (OSOM) vehicles. All oversize/overmass routes are noted for information only and are subject to separate approvals. Lane closure times are also subject to Road Occupancy Licence requirements.

For over dimensional vehicles, generally vehicles that are greater than 25m in length or 3,5m width require a pilot(s). Extremely long or wide vehicles will require an escort (fee payable). Permits will be applied for by the transport operator.

Oversize vehicles will be required at this site for the delivery of large plant and piling rigs. These deliveries will occur outside of peak hours. Contractors will manage their own permits.

Australia Ave between Figtree Dr and Sarah Durack Ave is currently height limited (5.1m) as noted on Figure 14 below.



Figure 14: Australia Ave height limit

Where vehicles are unable to be accommodated during the Special Event arrangements they are to be scheduled for access and/or egress from site outside of special event times.

There are no Traffic Control Signal changes proposed for this site. All existing traffic control is unchanged from existing approved CTMP implemented.

GLC requires two Tunnel Boring Machines to be removed from Sydney Olympic Park and transported to the Rosehill site to undertake tunnelling works towards Westmead for the Western Tunnelling Package. The delivery is forecasted to commence as of August 2024 through until end of September 2024. The TMB's will be delivered to the Rosehill site under over size over mass permits. Parking restrictions will need to be in place for some of the wider loads.

Each TBM consists of 28 individual components delivered in a specific sequence, with the initial phase completing the tunnelling between Rosehill and Sydney Olympic Park the TBM's will be deconstructed into the same size components as per original delivery and transported back to Rosehill to be rebuilt and relaunched towards Westmead.

The TBM's will be delivered in sections with the largest component being the two gripper shields at 6.99m Wide, 4.33m in Height and 130 tonne.

56 TBM components will be transported to the Rosehill site. There are 2 different routes to site that have been assessed. TBM Route 1 (Figure 15) is for the widest and heaviest loads and route 2 is the standard route for loads up to the size of the gantries. All movements will occur at night under approved OSOM permits and when required, ROL and Council permits.

- Route 1 – 14 loads

COMPONENTS: Cutterheads, Front Shields, Main Drives, Gripper shields, Inner telescopic shields, Outer telescopic shields & Tail skins

VIA: Herb Elliott Ave, Olympic Blvd, Sarah Durack Ave, Australia Ave, Homebush Bay Dr, right onto the M4 through the removed bollards, Silverwater Rd, Parramatta Rd, James Ruse Dr, Grand Ave, Colquhoun St, Unwin St, into Gate 1 at the Rosehill Site.

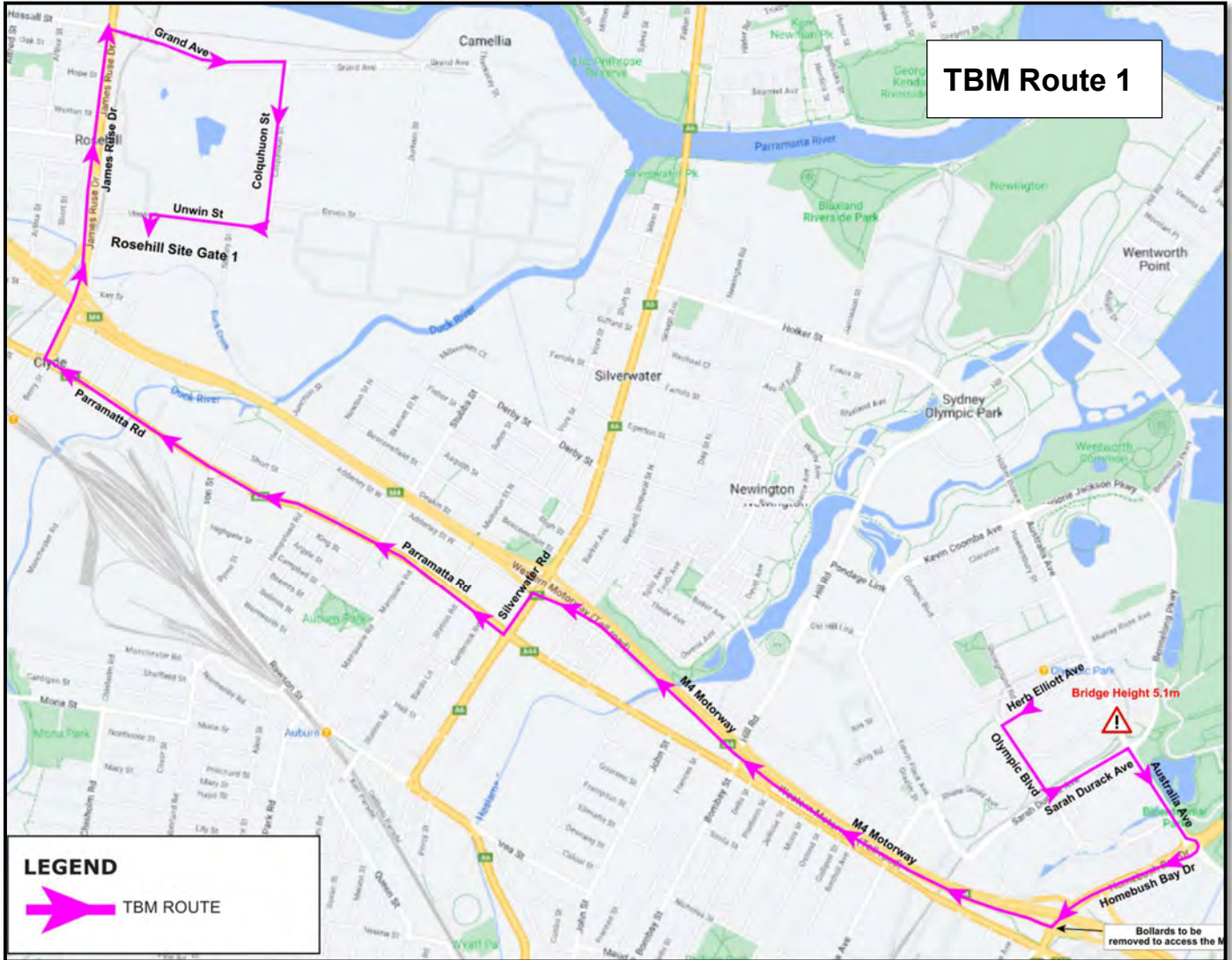


Figure 15: TBM Route 1

TBM Route 2 – 42 loads

COMPONENTS: Erector loads, Gantries and remaining small loads.

VIA: Herb Elliott Ave, Olympic Blvd, Sarah Durack Ave, Australia Ave, Homebush Bay Dr, left into the G loop onto the M4, James Ruse Dr, Grand Ave, Colquhoun St, Unwin St, into Gate 1 at the Rosehill Site.

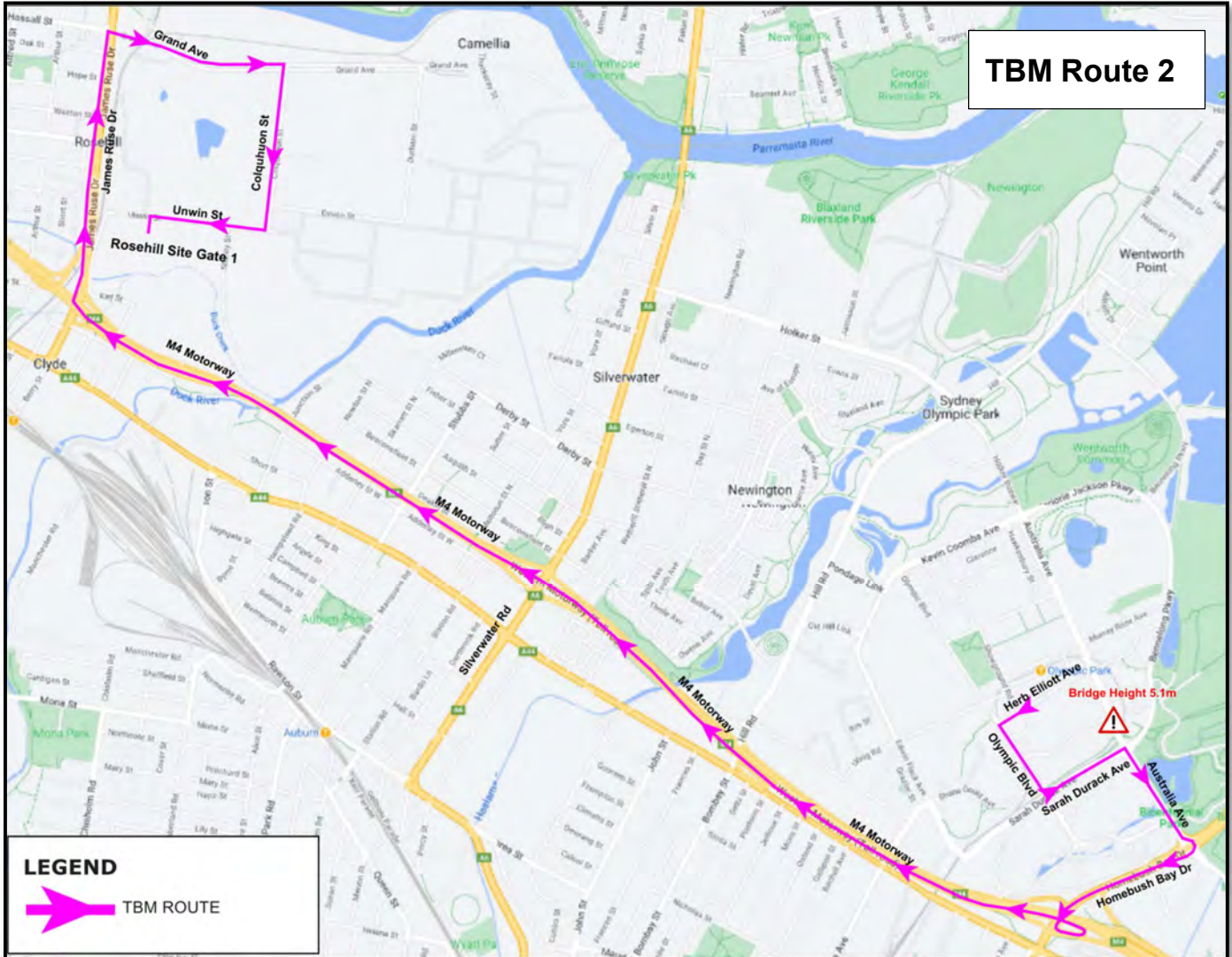


Figure 16: TBM Route 2

16 No. deliveries per TBM will require the bollards to be removed to enter the M4 under a right turn from Homebush Bay Dr Southbound and Traffic Control assistance once they reach the Rosehill area. User Pays Police and a tow truck are proposed to be onsite to allow for the removal of any vehicles that remain in the parking lane. The remaining loads do not require any additional assistance on top of the normal pilot vehicle associated with the movement.

8 COMMUNITY AND CONSULTATION

8.1 Stakeholders

Table 11 notes the consultation undertaken in the development of this Heavy Vehicle Local Road report. Appendix C - includes the comments received and GLC's responses to those comments.

Table 11: Stakeholder consultation

Stakeholder	Date	Consultation
TCG	05/10/2023 12/10/2023	Presentation
TTLG	28/09/2023	Presentation
Sydney Metro Project team	17/10/2023	Submission of HVLR report
CJP	17/10/2023	Submission of HVLR report
Sydney Olympic Park Authority	12/10/2023	Presentation
Department of Planning and Environment	Pending	Submission of HVLR report

In addition, to the stakeholders mentioned above, GLC will continue to work with AF JV to ensure traffic related activities are managed effectively and do not adversely effect the public. Weekly coordination meetings take place where both parties gain an understanding of upcoming activities. GLC will operate and access the site in the same way that AF JV does in order to minimise stakeholder impact.

GLC also have regular stakeholder meetings with the Sydney Olympic Park Authority – a copy of the most recent meeting minutes have been provided in appendix G.

8.2 Workforce communications

All personnel, including subcontractors, are required to attend a compulsory project and site induction before commencing any works on site. Similarly, visitors will be required to undertake a visitor's induction. This HVRL report will be included in the Construction Traffic Management Plan (CTMP) and will be included in the site induction for heavy vehicle drivers. A record of all attendees will be maintained.

Toolbox talks will be conducted and will be used to promote the safety and environmental performance including compliance with this report and the approved CTMP.

Table 12 notes the notifications to be provided to the local community and travelling public for the site operations works, associated with this CTMP.

Any enquiries, compliments or complaints will be directed to GLC's communications team via

- Information line 1800 612 173
- Email metrotunnels@transport.nsw.gov.au
- Mailing address Sydney Metro West, PO BOX K659, Haymarket, NSW 1240

Table 12: Proposed community notifications

Notification	Applicable?
Newsletters	Yes
Construction email updates	Yes
Fact sheets	Yes
Site signage	Yes
GLC website	Pending
Sydney Metro website	Pending

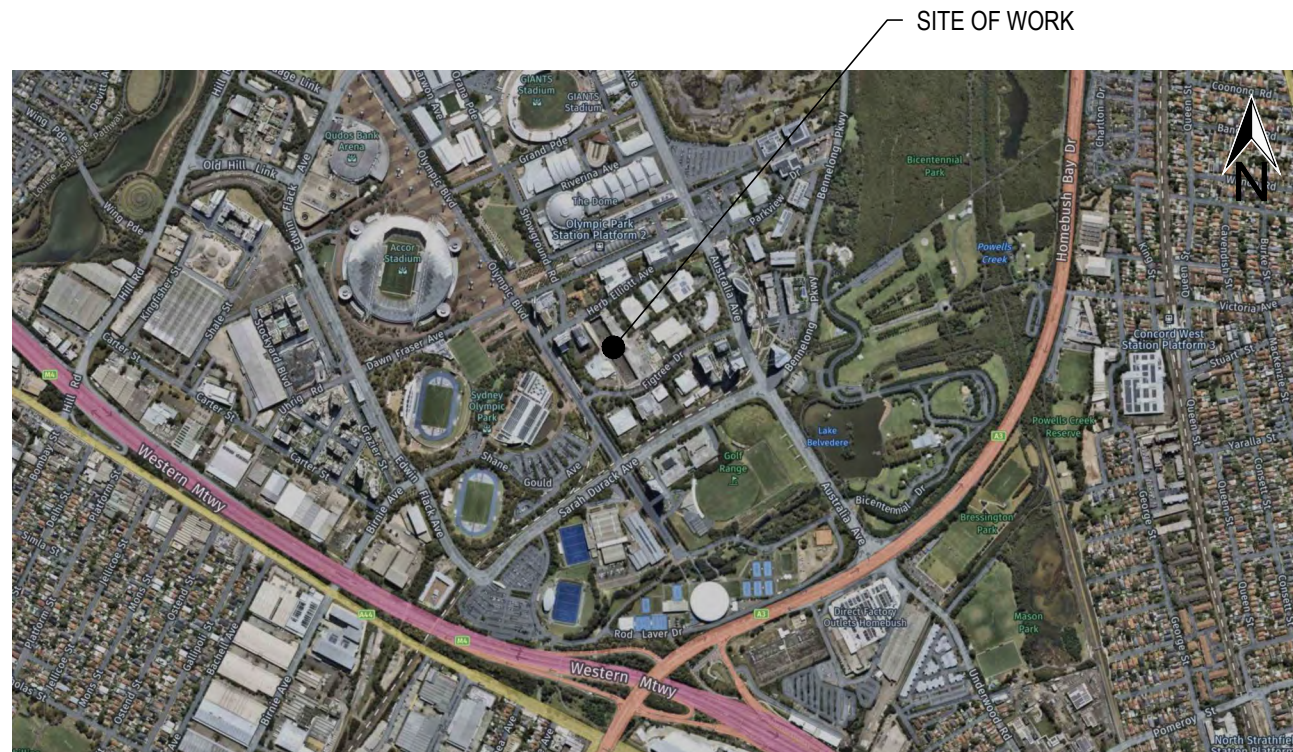
APPENDIX A – SWEPTPATH

CITY OF PARRAMATTA COUNCIL

SOPA SITE - SWEPT PATHS

SYDNEY METRO - WESTERN TUNNEL PACKAGE

SWEPT PATHS



LOCALITY PLAN
NOT TO SCALE

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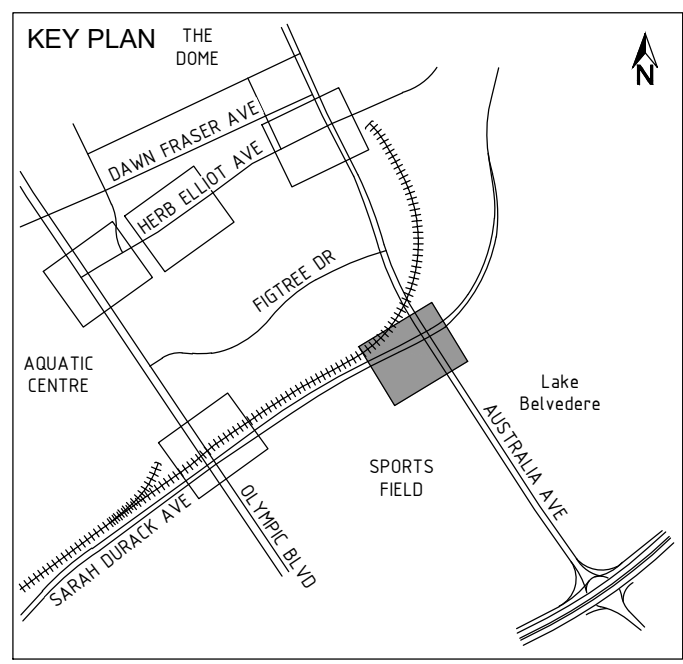
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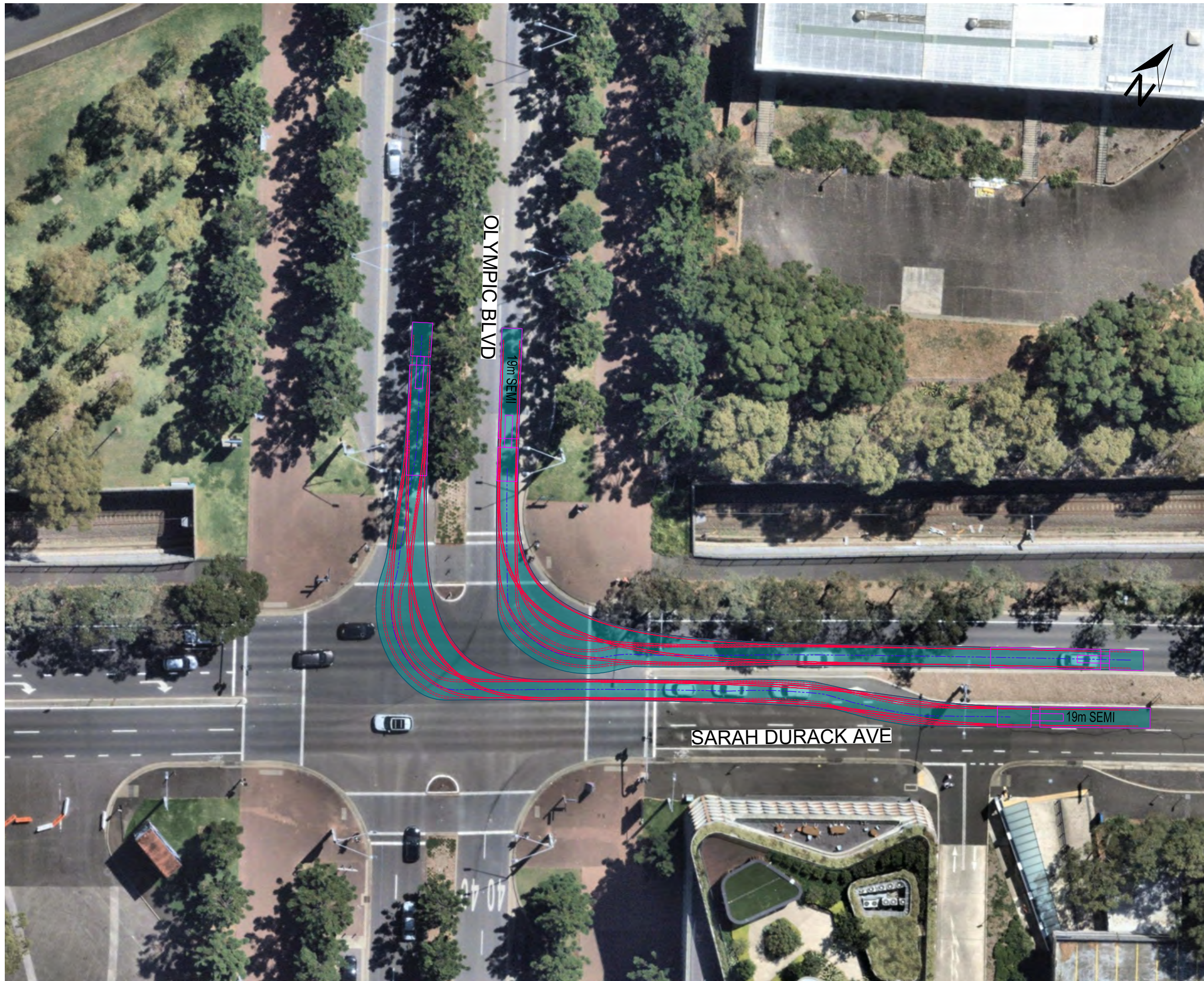
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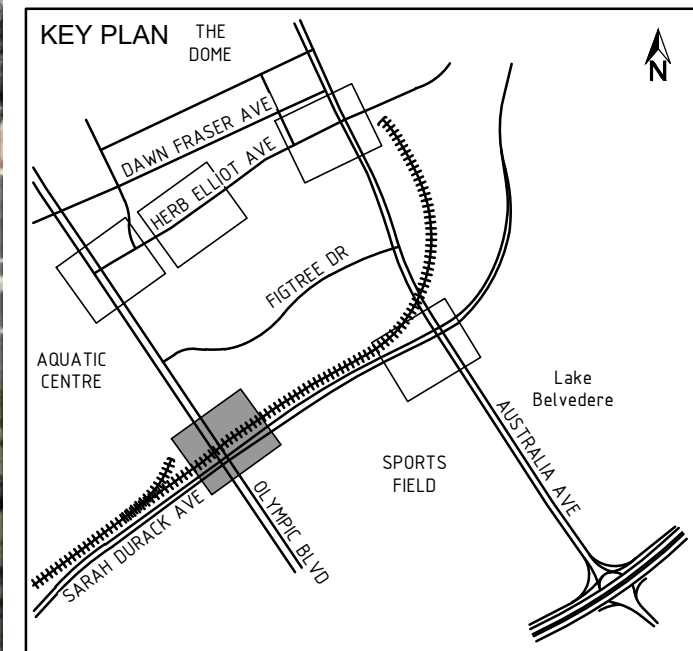
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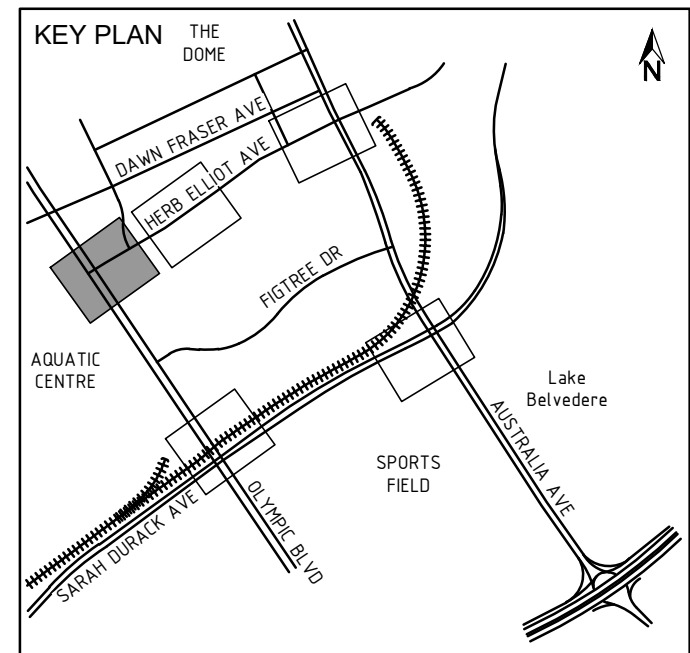
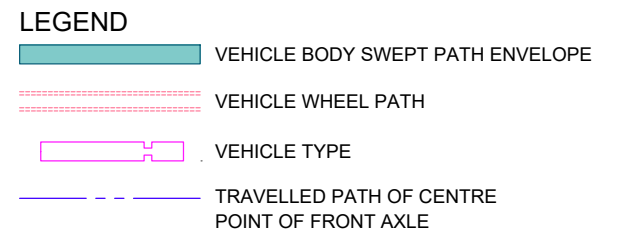
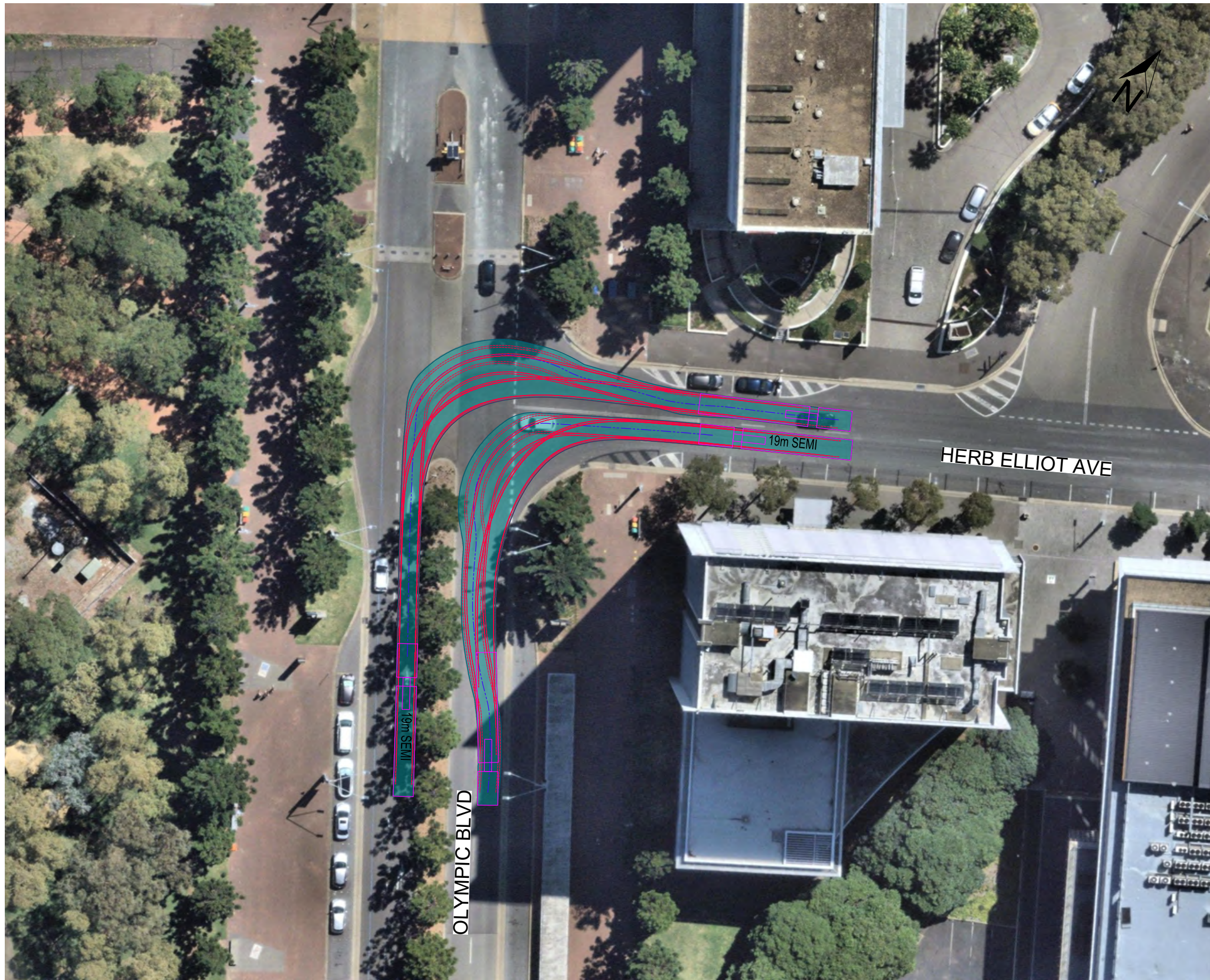


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EXTERNAL REFERENCE FILES		WVR No.	APPROVAL	SCALES ON A3 SIZE DRAWING	TITLE	NAME	DATE		
REV	DATE	AMENDMENT / REVISION DESCRIPTION		 CO-ORDINATE SYSTEM MGA ZONE 56	DRAWINGS / DESIGN PREPARED BY			RMS REGISTRATION No. ISSUE STATUS FOR CONSTRUCTION	
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B	07.11.23	PATHS REDONE TO SHOW WHEEL PATHS				DRAWN M.POMPLUN 08.11.23	DESIGN L.NEL 08.11.23	DESIGN CHECK J.COX 08.11.23	
C	08.11.23	PRESENTATION CHANGED AS PER REVIEWERS INSTRUCTION		HEIGHT DATUM AHD		DRG CHECK L.NEL 08.11.23			DESIGN CHECK J.COX 08.11.23



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50mm ON A3 SIZE ORIGINAL

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EXTERNAL REFERENCE FILES	REV DATE AMENDMENT / REVISION DESCRIPTION	WVR No. APPROVAL	SCALES ON A3 SIZE DRAWING
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	B 07.11.23 PATHS REDONE TO SHOW WHEEL PATHS		
	C 08.11.23 PRESENTATION CHANGED AS PER REVIEWERS INSTRUCTION		

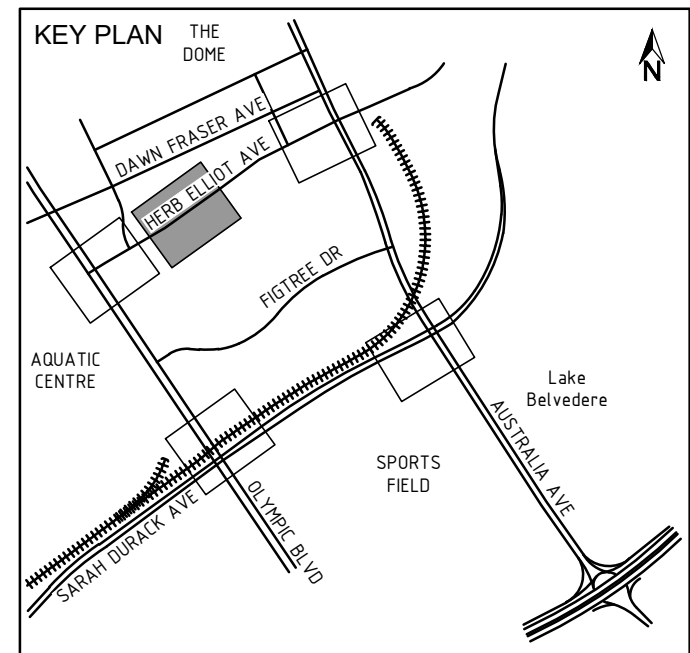
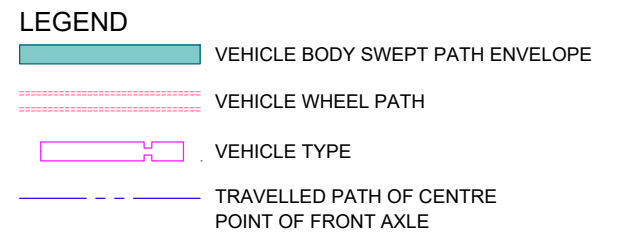
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CONTRACTOR	
CO-ORDINATE SYSTEM MGA ZONE 56	HEIGHT DATUM AHD

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TITLE	NAME	DATE
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DRG CHECK	L.NEL	08.11.23
DESIGN	L.NEL	08.11.23
DESIGN CHECK	J.COX	08.11.23
DESIGN MNGR		
PROJECT MNGR		

CLIENT

PREPARED FOR

CITY OF PARRAMATTA COUNCIL SOPA SITE - SWEEP PATHS 19m ARTICULATED VEHICLE INGRESS / EGRESS ROUTE		A3
SWEEP PATHS		SHEET 3 OF 16
RMS REGISTRATION No.		PART
ISSUE STATUS FOR CONSTRUCTION	EDMS No.	SHEET No. TS-02-1103
		ISSUE C



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50mm ON A3 SIZE ORIGINAL

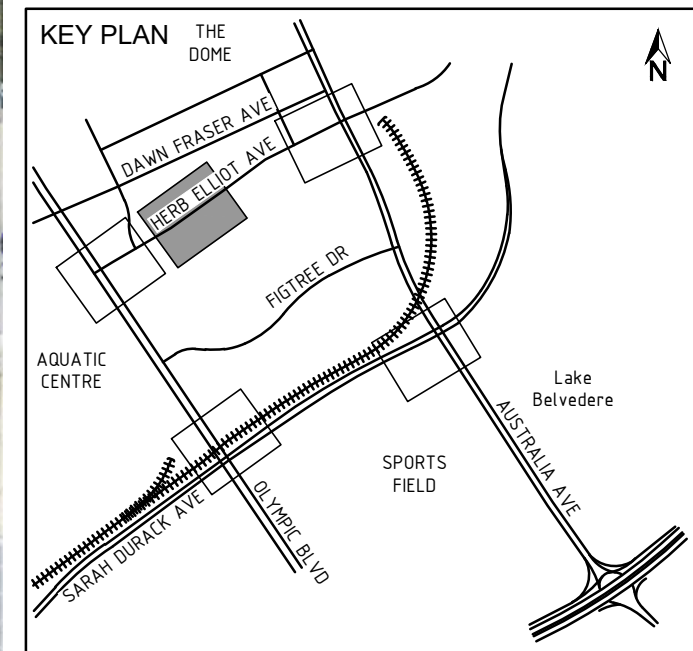
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EXTERNAL REFERENCE FILES		REV	DATE	AMENDMENT / REVISION DESCRIPTION	WVR No.	APPROVAL	SCALES ON A3 SIZE DRAWING		DRAWINGS / DESIGN PREPARED BY	
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		B	07.11.23	PATHS REDONE TO SHOW WHEEL PATHS			CO-ORDINATE SYSTEM MGA ZONE 56		CONTRACTOR	
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SHEET 4 OF 16



LEGEND

	VEHICLE BODY SWEEP PATH ENVELOPE
	VEHICLE WHEEL PATH
	VEHICLE TYPE
	TRAVELLED PATH OF CENTRE POINT OF FRONT AXLE



ACCEPTED FOR CONSTRUCTION

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50mm ON A3 SIZE ORIGINAL

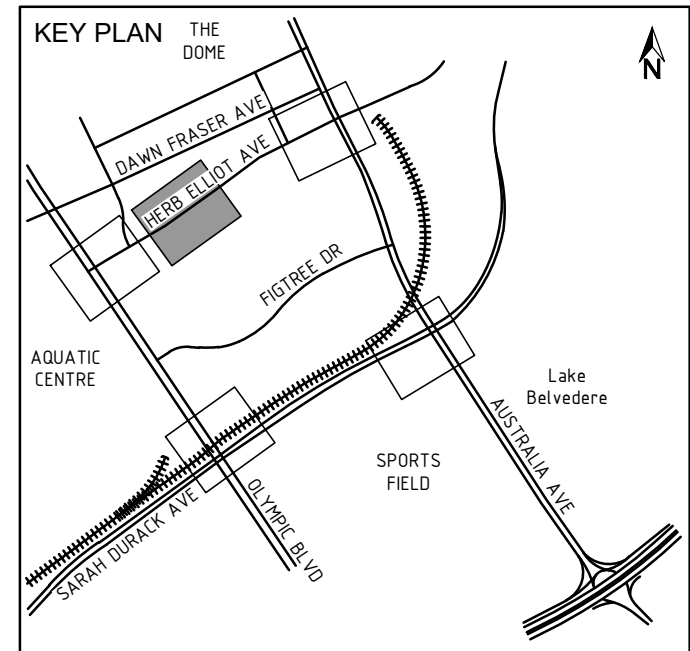
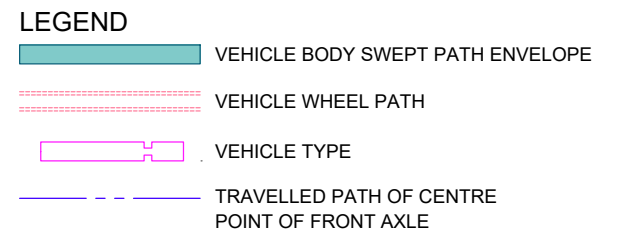
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DRAWINGS / DESIGN PREPARED BY	
CONTRACTOR	

PLOT DATE / TIME 08 Nov 2023 22:04:58	PLOT BY Mike	
TITLE	NAME	DATE
DRAWN	M.POMPLUN	08.11.23
DRG CHECK	L.NEL	08.11.23
DESIGN	L.NEL	08.11.23
DESIGN CHECK	J.COX	08.11.23
DESIGN MNGR		
PROJECT MNGR		

CLIENT	
PREPARED FOR	

CITY OF PARRAMATTA COUNCIL SOPA SITE - SWEEP PATHS 19m ARTICULATED VEHICLE INGRESS / EGRESS ROUTE		A3
SWEPT PATHS		SHEET 5 OF 16
RMS REGISTRATION No.		PART
ISSUE STATUS FOR CONSTRUCTION	EDMS No.	SHEET No. TS-02-1105
		ISSUE C



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50mm ON A3 SIZE ORIGINAL

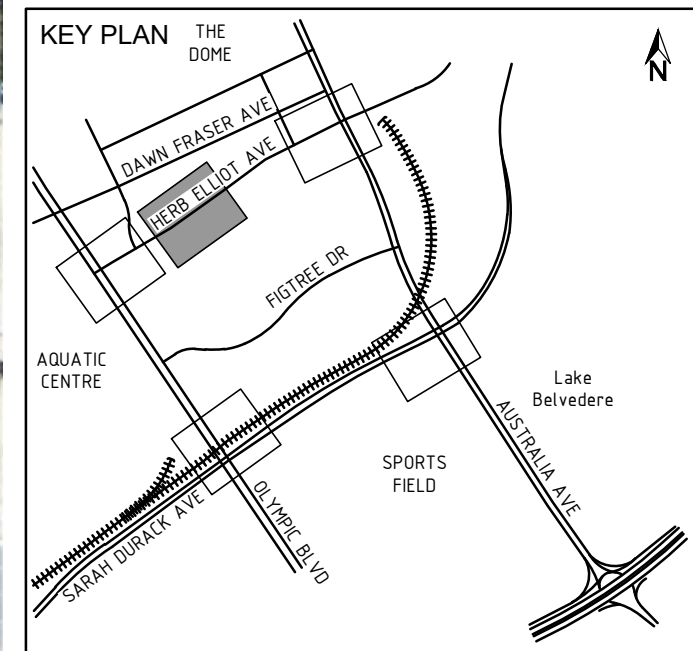
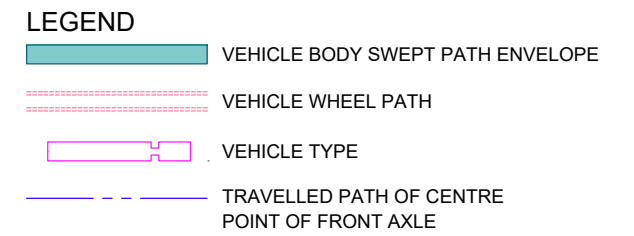
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B	07.11.23	PATHS REDONE TO SHOW WHEEL PATHS		DESIGN	L.NEL	08.11.23
C	08.11.23	PRESENTATION CHANGED AS PER REVIEWERS INSTRUCTION		DESIGN CHECK	J.COX	08.11.23
P:\0089_1116_Book_a3 SGA-SP101_SGPA S-SE-FRM-500 - SGPA S-SE-FRM-500 - SGPA S-SE-FRM-500 - SGPA		SCALES ON A3 SIZE DRAWING	DRAWINGS / DESIGN PREPARED BY	DESIGN MNGR		
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DRG CHECK	L.NEL	08.11.23
DESIGN	L.NEL	08.11.23
DESIGN CHECK	J.COX	08.11.23
DESIGN MNGR		
PROJECT MNGR		

PREPARED FOR	sydney METRO
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CITY OF PARRAMATTA COUNCIL SOPA SITE - SWEEP PATHS 19m ARTICULATED VEHICLE EVENT DAY ROUTE		A3
SWEEP PATHS		SHEET 6 OF 16
RMS REGISTRATION No.		PART
ISSUE STATUS FOR CONSTRUCTION	EDMS No.	SHEET No. TS-02-1106
		ISSUE C



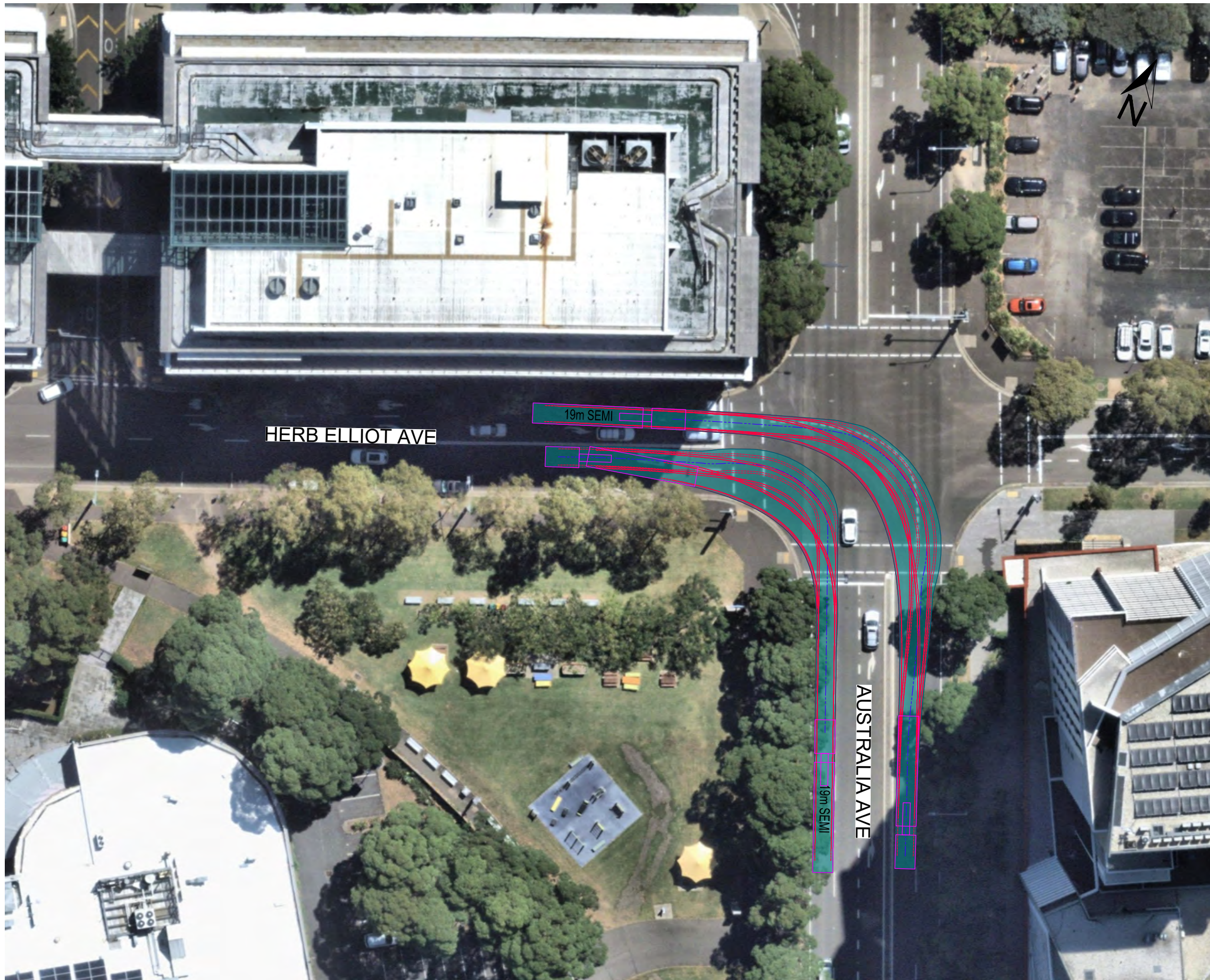
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50mm ON A3 SIZE ORIGINAL

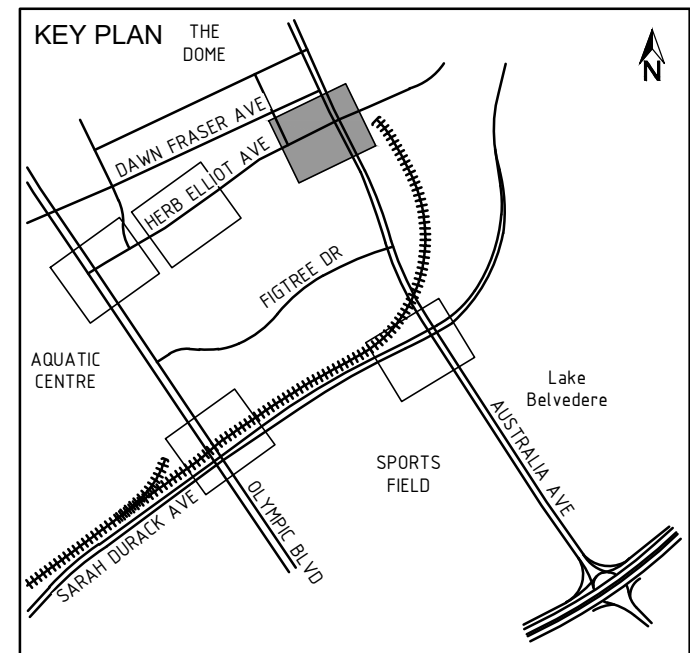
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EXTERNAL REFERENCE FILES		REV	DATE	AMENDMENT / REVISION DESCRIPTION	WVR No.	APPROVAL	SCALES ON A3 SIZE DRAWING		DRAWINGS / DESIGN PREPARED BY	
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								ISSUE STATUS		FOR CONSTRUCTION
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SHEET 7 OF 16



LEGEND

	VEHICLE BODY SWEEP PATH ENVELOPE
	VEHICLE WHEEL PATH
	VEHICLE TYPE
	TRAVELLED PATH OF CENTRE POINT OF FRONT AXLE



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	B	07.11.23	PATHS REDONE TO SHOW WHEEL PATHS
	C	08.11.23	PRESENTATION CHANGED AS PER REVIEWERS INSTRUCTION

WVR No.	APPROVAL	SCALES ON A3 SIZE DRAWING	DRAWINGS / DESIGN PREPARED BY
CO-ORDINATE SYSTEM	HEIGHT DATUM	CONTRACTOR	
MGA ZONE 56	AHD		

PLOT DATE / TIME	PLOT BY	CLIENT
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TITLE	NAME	DATE
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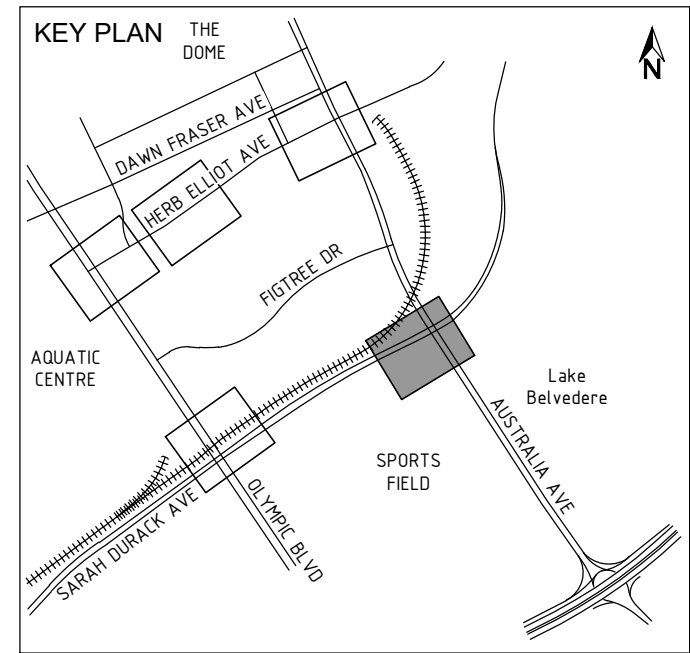
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CITY OF PARRAMATTA COUNCIL SOPA SITE - SWEEP PATHS 19m ARTICULATED VEHICLE EVENT DAY ROUTE		A3
SWEEP PATHS		SHEET 8 OF 16
RMS REGISTRATION No.		PART
ISSUE STATUS	EDMS No.	SHEET No.
FOR CONSTRUCTION		TS-02-1108
		ISSUE C

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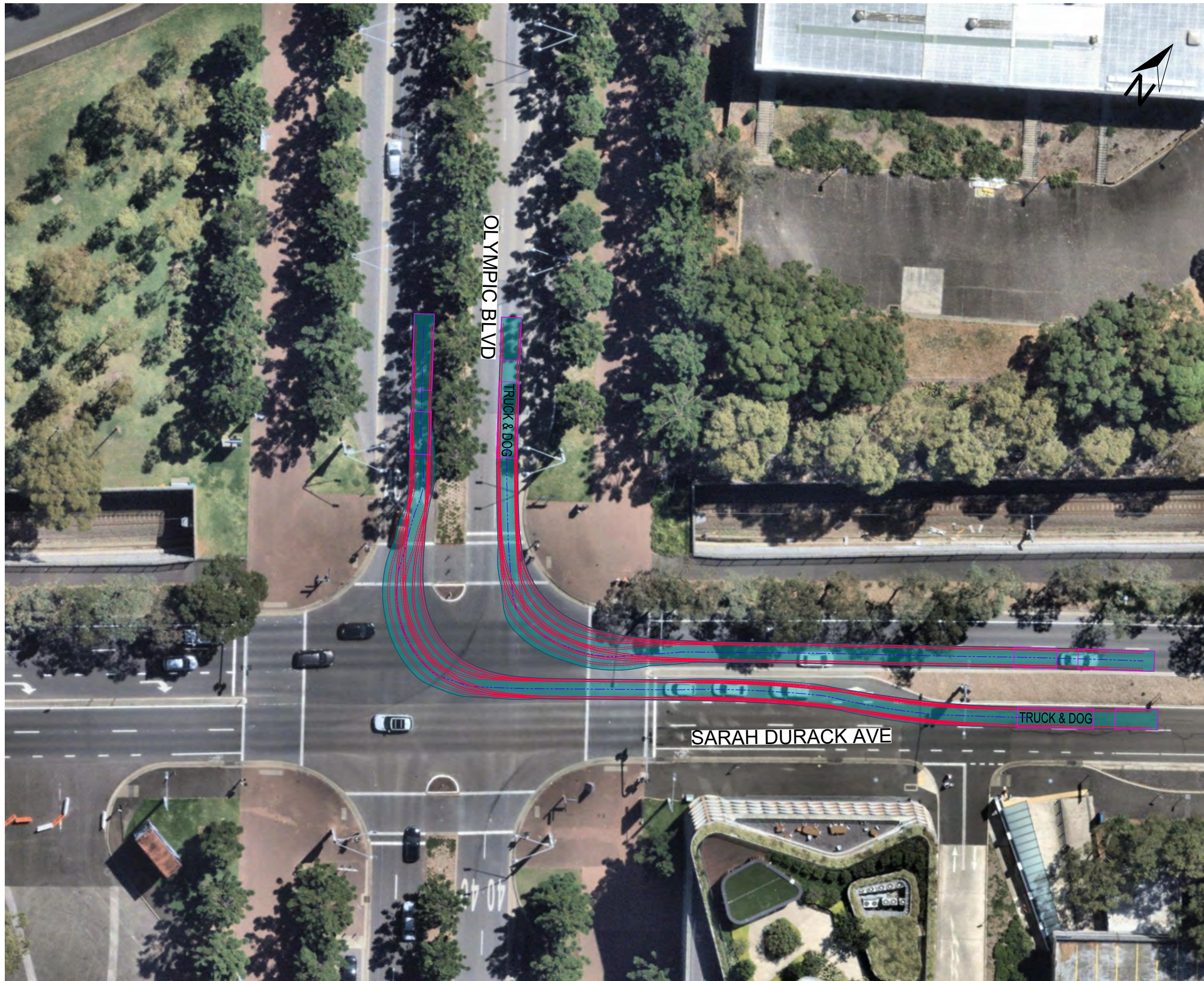


LEGEND	
	VEHICLE BODY SWEEP PATH ENVELOPE
	VEHICLE WHEEL PATH
	VEHICLE TYPE
	TRAVELLED PATH OF CENTRE POINT OF FRONT AXLE



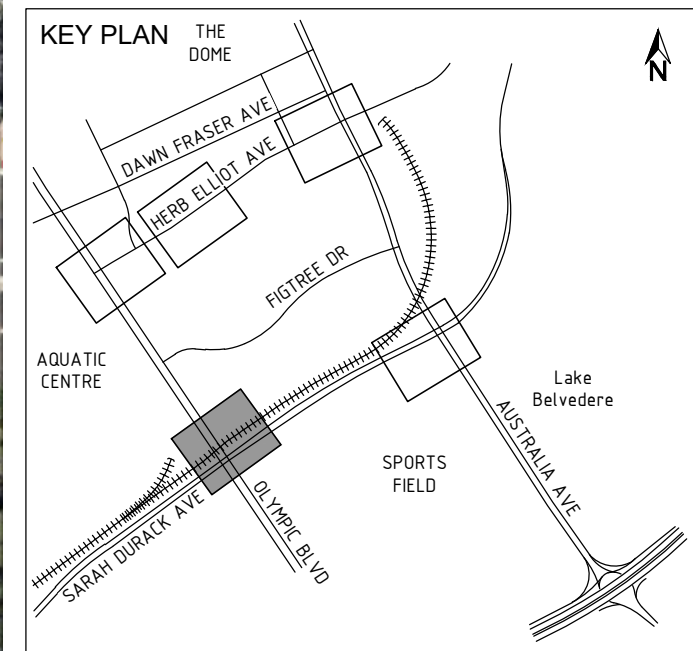
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C	08.11.23	PRESENTATION CHANGED AS PER REVIEWERS INSTRUCTION		HEIGHT DATUM AHD		DESIGN	L.NEL	DATE	08.11.23
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LEGEND

	VEHICLE BODY SWEEP PATH ENVELOPE
	VEHICLE WHEEL PATH
	VEHICLE TYPE
	TRAVELLED PATH OF CENTRE POINT OF FRONT AXLE



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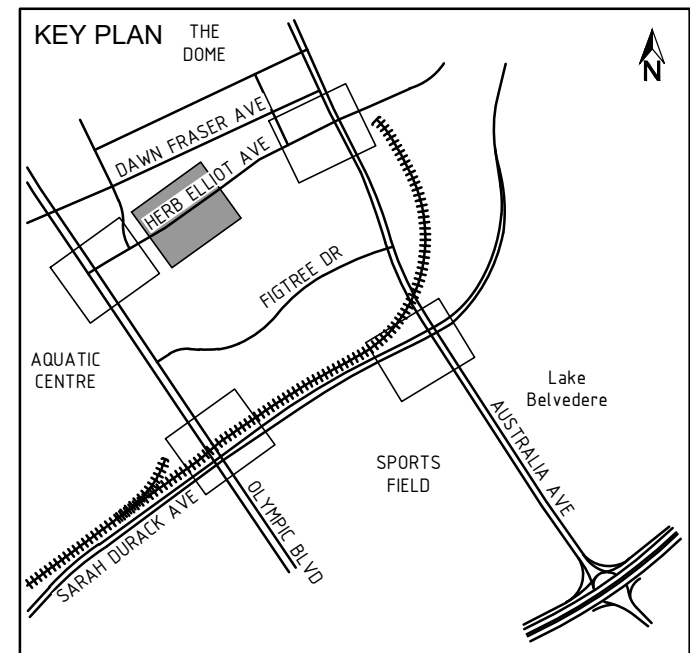
SHEET 10 OF 16

PART

ISSUE



- LEGEND**
- VEHICLE BODY SWEEP PATH ENVELOPE
 - VEHICLE WHEEL PATH
 - VEHICLE TYPE
 - TRAVELLED PATH OF CENTRE POINT OF FRONT AXLE



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EXTERNAL REFERENCE FILES	REV DATE AMENDMENT / REVISION DESCRIPTION	WVR No. APPROVAL	SCALES ON A3 SIZE DRAWING
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	B 07.11.23 PATHS REDONE TO SHOW WHEEL PATHS		
	C 08.11.23 PRESENTATION CHANGED AS PER REVIEWERS INSTRUCTION		
CO-ORDINATE SYSTEM MGA ZONE 56	HEIGHT DATUM AHD	DRAWINGS / DESIGN PREPARED BY	

PLOT DATE / TIME 08 Nov 2023 22:08:49	PLOT BY Mike	CLIENT
TITLE	NAME	DATE
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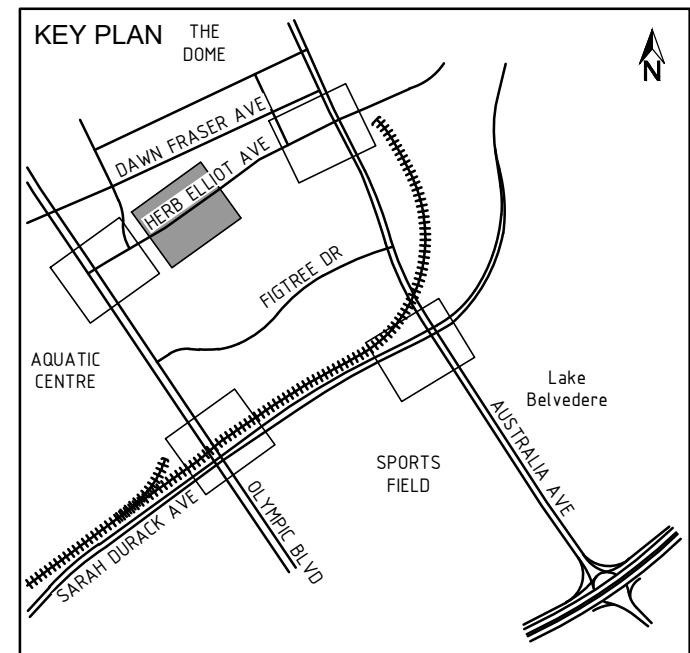
CONTRACTOR		
PREPARED FOR		CITY OF PARRAMATTA COUNCIL SOPA SITE - SWEEP PATHS TRUCK AND DOG INGRESS / EGRESS ROUTE

RMS REGISTRATION No.		SHEET 12 OF 16	
ISSUE STATUS FOR CONSTRUCTION	EDMS No.	SHEET No. TS-02-1112	ISSUE C

A3



LEGEND	
	VEHICLE BODY SWEEP PATH ENVELOPE
	VEHICLE WHEEL PATH
	VEHICLE TYPE
	TRAVELLED PATH OF CENTRE POINT OF FRONT AXLE



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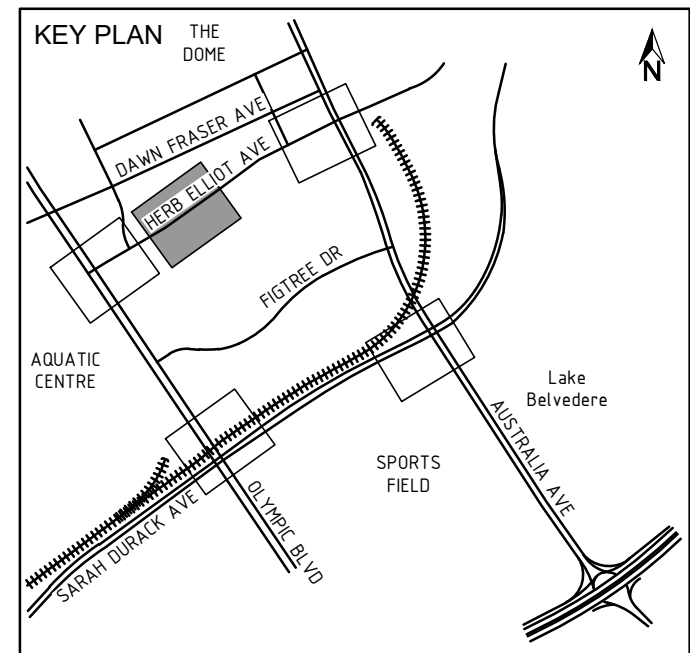
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50mm ON A3 SIZE ORIGINAL

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DRG CHECK	L.NEL	08.11.23																												
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DESIGN CHECK	J.COX	08.11.23																												
DESIGN MNGR																														
PROJECT MNGR																														
	B	07.11.23	PATHS REDONE TO SHOW WHEEL PATHS			CO-ORDINATE SYSTEM MGA ZONE 56	CONTRACTOR	PREPARED FOR	ISSUE STATUS FOR CONSTRUCTION																					
	C	08.11.23	PRESENTATION CHANGED AS PER REVIEWERS INSTRUCTION			HEIGHT DATUM AHD			EDMS No.	SHEET No. TS-02-1113	ISSUE C																			



- LEGEND**
- VEHICLE BODY SWEEP PATH ENVELOPE
 - VEHICLE WHEEL PATH
 - VEHICLE TYPE
 - TRAVELLED PATH OF CENTRE POINT OF FRONT AXLE



ACCEPTED FOR CONSTRUCTION

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50mm ON A3 SIZE ORIGINAL

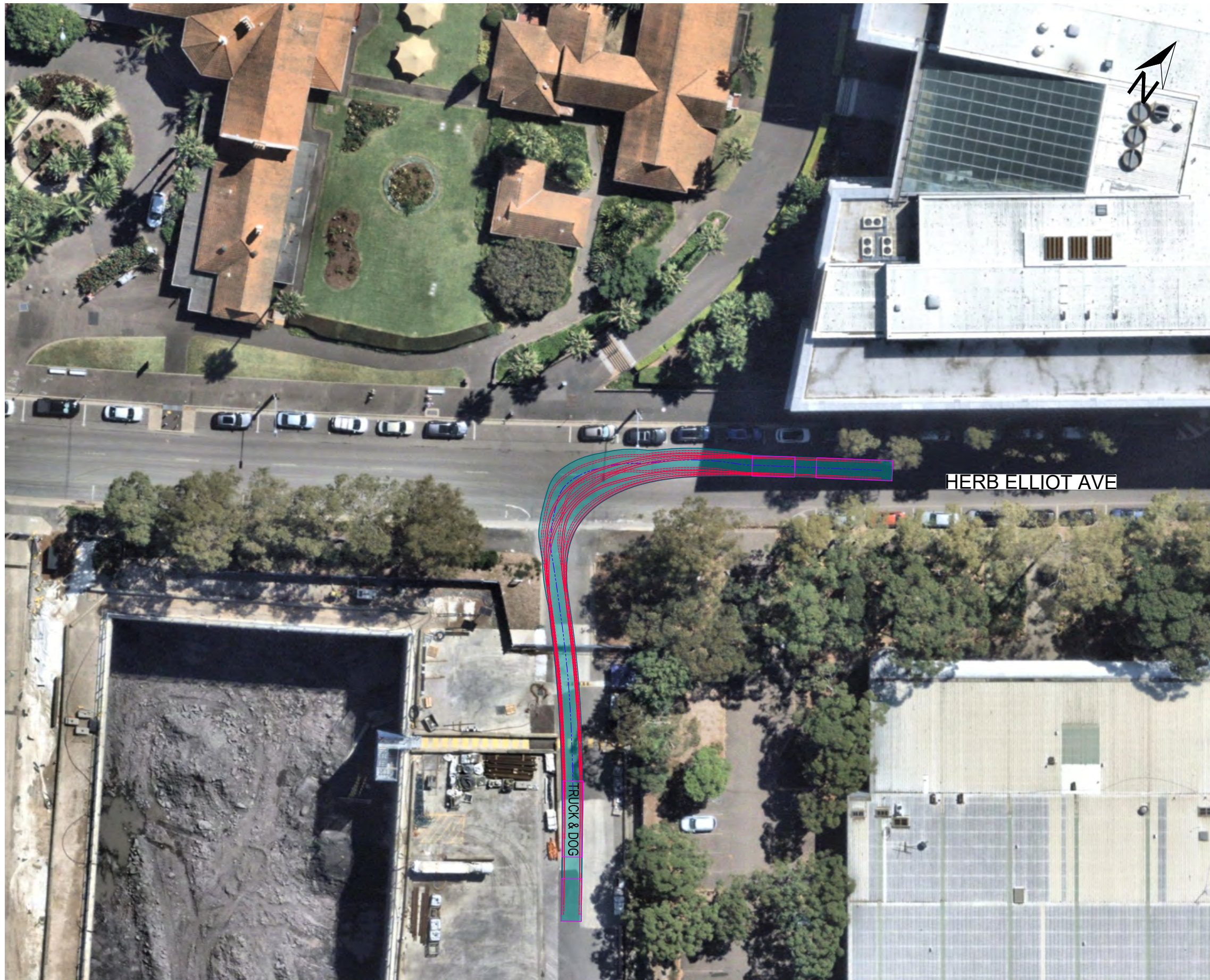
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	B	07.11.23	PATHS REDONE TO SHOW WHEEL PATHS
	C	08.11.23	PRESENTATION CHANGED AS PER REVIEWERS INSTRUCTION

WVR No.	APPROVAL	SCALES ON A3 SIZE DRAWING	DRAWINGS / DESIGN PREPARED BY
CONTRACTOR		CO-ORDINATE SYSTEM MGA ZONE 56	HEIGHT DATUM AHD

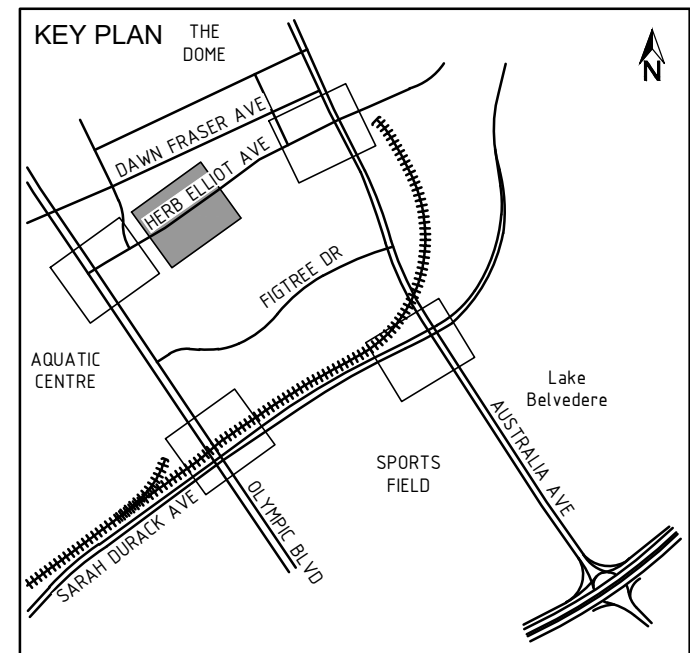
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DESIGN	L.NEL	08.11.23	
DESIGN CHECK	J.COX	08.11.23	
DESIGN MNGR			
PROJECT MNGR			

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PREPARED FOR	

CITY OF PARRAMATTA COUNCIL SOPA SITE - SWEEP PATHS TRUCK AND DOG EVENT DAY ROUTE		A3
SWEEP PATHS		SHEET 14 OF 16
RMS REGISTRATION No.		PART
ISSUE STATUS FOR CONSTRUCTION	EDMS No.	SHEET No. TS-02-1114
		ISSUE C



LEGEND	
	VEHICLE BODY SWEEP PATH ENVELOPE
	VEHICLE WHEEL PATH
	VEHICLE TYPE
	TRAVELLED PATH OF CENTRE POINT OF FRONT AXLE



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50mm ON A3 SIZE ORIGINAL

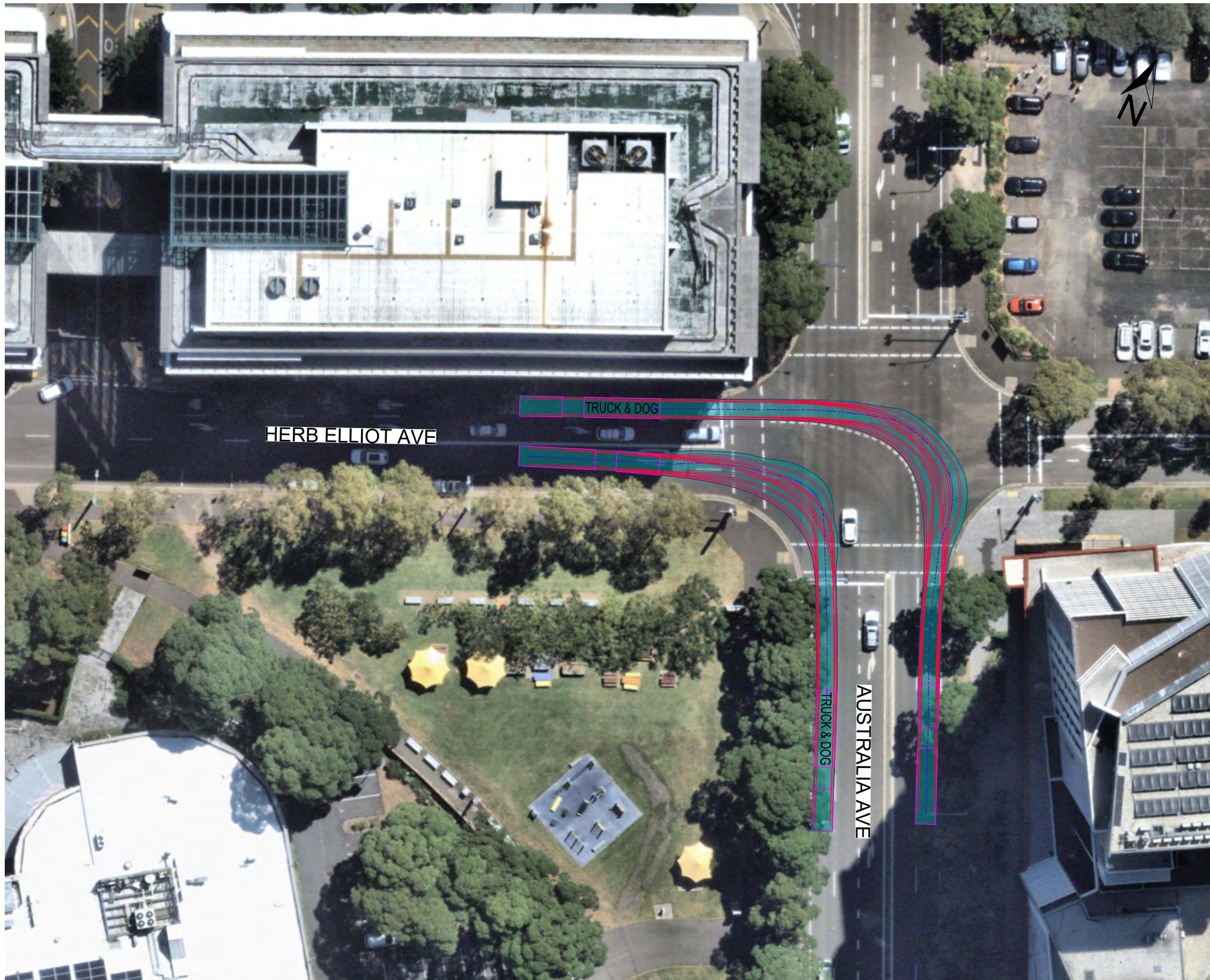
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	C 08.11.23 PRESENTATION CHANGED AS PER REVIEWERS INSTRUCTION		
CO-ORDINATE SYSTEM MGA ZONE 56	HEIGHT DATUM AHD	DRAWINGS / DESIGN PREPARED BY	

DRAWINGS / DESIGN PREPARED BY	
CONTRACTOR	

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DRG CHECK	L.NEL	08.11.23	
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DESIGN CHECK	J.COX	08.11.23	
DESIGN MNGR			
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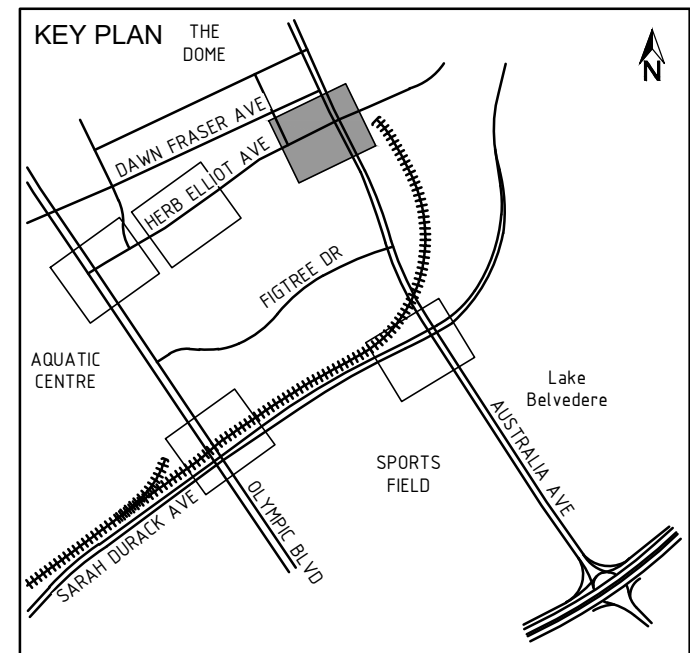
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CITY OF PARRAMATTA COUNCIL SOPA SITE - SWEEP PATHS TRUCK AND DOG EVENT DAY ROUTE		A3
SWEEP PATHS		SHEET 15 OF 16
RMS REGISTRATION No.		PART
ISSUE STATUS FOR CONSTRUCTION	EDMS No.	SHEET No. TS-02-1115
		ISSUE C



LEGEND

	VEHICLE BODY SWEEP PATH ENVELOPE
	VEHICLE WHEEL PATH
	VEHICLE TYPE
	TRAVELLED PATH OF CENTRE POINT OF FRONT AXLE



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		HEIGHT DATUM AHD		SWEEP PATHS		
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		SCALE		PROJECT MNGR		
		CONTRACTOR		PREPARED FOR		
		CO-ORDINATE SYSTEM MGA ZONE 56		CITY OF PARRAMATTA COUNCIL SOPA SITE - SWEEP PATHS TRUCK AND DOG EVENT DAY ROUTE		
		HEIGHT DATUM AHD		SWEEP PATHS		
				RMS REGISTRATION No.		
				ISSUE STATUS FOR CONSTRUCTION		
				EDMS No.		
				SHEET No. TS-02-1116		
				ISSUE C		

A3

SHEET 16 OF 16

PART

ISSUE

APPENDIX C - STAKEHOLDER CONSULTATION

REVIEW COMMENTS SHEET

DOCUMENT NO.	TITLE	VER	STATUS	NO.	DATE	COMPANY	RAISED BY	REVIEW DOC. NO.*	DOCUMENT REF*	DEED REF*	COMMENTS / RESPONSE	COMMENT CATEGORY*	CLOSED OUT
SMWSTWTP-GLO-OLP-TF-RPT-000001	Sydney Metro West - WTP - Heavy Vehicle Local Road Report - Sydney Olympic Park	B.01	S3	01	20/10/2023	SCO	DCROWLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Clause 6	-	A more detailed site plan is required showing how heavy vehicles will manoeuvre around the site and how they interface with CTP	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Clause 6	-	See response below	Observation	N
				01.01	2/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Clause 6	-	Section 6 has been expanded and now includes two new figures detailing this.	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Clause 6	-	-	Observation	N
				02	20/10/2023	SCO	DCROWLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Figure 7.3	-	Why is the vehicle egress route not via Figtree Dr to mirror the CTP routes? The proposed egress route will increase the number of heavy vehicles on Herb Elliot Av which will adversely impact the businesses in the area, requiring additional consultation.	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Figure 7.3	-	See response below	Observation	N
				02.01	2/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Figure 7.3	-	GLC do not have possession of that end of the site. That end of the site belongs to AFJV.	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Figure 7.3	-	-	Observation	N
				03	20/10/2023	SCO	DCROWLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Figure 7.3	-	The proposal to have the existing Herb Elliott Av gate operate as both an entry and exit (split approach) has not been adequately assessed.	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Figure 7.3	-	See response below	Observation	N
				03.01	2/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Figure 7.3	-	A TGS has been developed for this operation that has been reviewed by an independent road safety auditor. GLC will ensure deliveries are coordinated so that vehicles can be turned around safely inside the site. Vehicles will leave the site under traffic control instructions.	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Figure 7.3	-	-	Observation	N
				04	20/10/2023	SCO	DCROWLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Clause 7.7	-	All oversize/overmass routes are noted for information only and are subject to separate approvals. Lane closure times are also subject to Road Occupancy Licence requirements.	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Clause 7.7	-	See response below	Observation	N
				04.01	2/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Clause 7.7	-	Agreed	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Clause 7.7	-	Reference to this has been made within the document in Section 7.7.	Observation	N
				05	20/10/2023	SCO	DCROWLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Clause 7.3	-	The total numbers for the site traffic generation (both contractors) should be identified to demonstrate compliance with the EIS volumes.	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Clause 7.3	-	See response below	Observation	N
				05.01	2/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Clause 7.3	-	This has been added	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Clause 7.3	-	-	Observation	N
				06	20/10/2023	SCO	DCROWLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Clause 7.3	-	The AFJV traffic generation figures do not match the volumes as per their approved CTMP	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Clause 7.3	-	See response below	Observation	N
				06.01	2/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Clause 7.3	-	This has been updated	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Clause 7.3	-	Refer to section 7.3	Observation	N
				07	20/10/2023	SCO	DCROWLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Table 3	-	The 'AFJV Heavy' column is incorrectly labelled. Also note the AM figures are incorrect.	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Table 3	-	See response below	Observation	N
				07.01	2/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Table 3	-	This has been updated	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Table 3	-	Refer to section 7.3	Observation	N
				08	20/10/2023	SCO	DCROWLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Clause 8	-	Further consultation with impacted residents will be required due to the additional heavy vehicles on Herb Elliot Av. Particularly with the hotels in the area.	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Clause 8	-	Refer response below	Observation	N
				08.01	2/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Clause 8	-	GLC have developed a comms plan and strategy - details have been included within the CTMP.	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Clause 8	-	Refer to appendix E	Observation	N
				09	20/10/2023	SCO	DCROWLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Appendices	-	Appendices are not clearly labelled / defined.	Observation	N

DOCUMENT NO.	TITLE	VER	STATUS	NO.	DATE	COMPANY	RAISED BY	REVIEW DOC. NO.²	DOCUMENT REF¹	DEED REF¹	COMMENTS / RESPONSE	COMMENT CATEGORY¹	CLOSED OUT
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Appendices	-	Refer to response below	Observation	N
				09.01	2/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Appendices	-	This has been updated	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Appendices	-	-	Observation	N
				10	20/10/2023	SCO	DCROWLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Appendix	-	The TCG presentation is not required in a HVLR. Just add a sentence to Clause 8	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Appendix	-	Refer to response below	Observation	N
				10.01	2/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Appendix	-	Noted - removed from document and referenced in Clause 8.	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Appendix	-	-	Observation	N
				14	30/10/2023	PAR	BSALEH	SMWSTWTP-GLO-OLP-TF-RPT-000001			A new 5t Load Limit restriction was installed in Bennelong Parkway at the bridge over Haslams Creek. This has been installed due to the poor structural condition of the bridge. Passed on to Metro as precaution although it doesn't appear any Metro construction vehicles are using the bridge	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001			Refer to response below	Observation	N
				14.01	2/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001			Noted - we are not using Bennelong Parkway as part of our heavy haulage route.	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001			-	Observation	N
				15	31/10/2023	SMD	SLEPRE	SMWSTWTP-GLO-OLP-TF-RPT-000001	Throughout	NA	"Error! Bookmark not defined" "Error! Reference source not found" present throughout document, please amend all.	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Throughout	NA	Refer to response below	Observation	N
				15.01	2/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Throughout	NA	Updated	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Throughout	NA	-	Observation	N
				15.01.01	6/11/2023	SMD	SLEPRE				I understand there is a current glitch, if this is resolved with the CTMP please amend with the HVLR also	Observation	N
					9/11/2023	GLO	DKELLY				The glitch has been fixed within both documents.	Observation	N
				16	1/11/2023	SOA	VMCGHIE	SMWSTWTP-GLO-OLP-TF-RPT-000001	Page 15, Section 5.5		Section 5.5 comments that the indented bays on Olympic Boulevard is for parking, which is not accurate. It should instead reflect that there is a mixture of timed parking and bus bays and that Sydney Olympic Park Authority does not approve trucks laying up in the indented bays which are intended for bus parking.	Potential Non-Compliance	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Page 15, Section 5.5		Refer to comment below	Potential Non-Compliance	N
				16.01	2/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Page 15, Section 5.5		Updated	Potential Non-Compliance	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Page 15, Section 5.5		Refer to Section 5.5 "There is a mixture of timed parking and bus bays along the route. The parking is reserved for the public and should no time be used to stable heavy vehicles"	Potential Non-Compliance	N
				17	1/11/2023	SOA	VMCGHIE	SMWSTWTP-GLO-OLP-TF-RPT-000001	Page 16, Section 6.1.3		SOFA requires that GLC provide advance notice (as far in advance as possible) regarding any vehicle movements after hours/overnight to enable resident notification and coordinate with any late-night finishes of events, particularly music festivals, which will bring increased pedestrian and vehicle movements. Can this be reflected in this section?	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Page 16, Section 6.1.3		Refer to the below comment	Observation	N
				17.01	2/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Page 16, Section 6.1.3		Added	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Page 16, Section 6.1.3		Refer to section 6.1.4 "GLC will attend a monthly meeting with Sydney Olympic Park committee to discuss any upcoming events and the impact GLC traffic operations may have on them."	Observation	N
				19	1/11/2023	SMD	AHENDY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Section 6.1.4	NA	Section 6.1.4 Managing cumulative impacts, currently states: "No works have been identified near the Sydney Olympic Park work site" What about cumulative impacts from AFJC / CTP contractor? Suggest a response is provided acknowledging AFJC / CTP contractor will be continuing to undertake construction activities. No changes to Sections 6.1.4.1 or 6.1.4.2 required	Observation	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Section 6.1.4	NA	Refer to the below comment	Observation	N
				19.01	2/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Section 6.1.4	NA	Updated	Observation	N

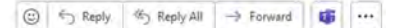
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					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Section 6.1.4	NA	Refer to section 6.1.4 "No works have been identified outside the Sydney Olympic Park work site. To ensure that GLC are aware of other projects and/ or impacts, GLC will attend the Traffic Control Group (TCG) and Traffic and Transport Liaison Group (TTLG) forums. Work inside the site and access using haulage routes will be coordinated with Sydney Olympic Park and AFJV to mitigate impact. GLC will attend a monthly meeting with Sydney Olympic Park committee to discuss any upcoming events and the impact GLC traffic operations may have on them"	Observation	N
				19.01.01	6/11/2023	SMD	SLEPRE				A clear sentence acknowledging CTP works was to be included into Section 6.1.4, this has not been completed. Please include text into the section in line with Andrew's comment clearly stating that CTP will be continuing to undertake construction activities at the same time as WTP works.	Observation	N
					9/11/2023	GLO	DKELLY				Added the below to Section 6.1.4 "AFJV will still be present at the Metro Sydney Olympic Park Site and will retain Principal Contractor of a part of the site where they have ongoing construction operations."	Observation	N
				19.01.02	6/11/2023	SMD	AHENDY				No changes appear to have been made. Please provide a clear statement acknowledging CTP will be continuing to undertake construction activities at the same time as WTP works.	Observation	N
					9/11/2023	GLO	DKELLY				Refer to above comment.	Observation	N
				21	1/11/2023	SMD	AHENDY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Section 7.3, Table 3	Schedule D4	It is noted in Table 3 within Section 7.3 that GLC are proposing to undertake the full number of heavy vehicle movements per hour as shown in the EIS. Is it known what numbers AFJV are currently running per hour? i.e. with GLC's heavy vehicle movements, what is the total expected to be compared with those shown in the EIS?	Potential Non-Compliance	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Section 7.3, Table 3	Schedule D4	Refer to the below comment	Potential Non-Compliance	N
				21.01	2/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Section 7.3, Table 3	Schedule D4	Updated	Potential Non-Compliance	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	Section 7.3, Table 3	Schedule D4	-	Potential Non-Compliance	N
				21.01.01	6/11/2023	SMD	AHENDY				Mostly addressed, however appears to be a formatting error on Table 4 GLC Site Operations Heavy Vehicle Movements per hour, where the HV movements row is not shown. Please update table to include this row.	Potential Non-Compliance	N
					9/11/2023	GLO	DKELLY				All tables regarding heavy vehicle movements have been updated following discussion with Sydney Metro and CJP.	Potential Non-Compliance	N
				22	1/11/2023	SMD	SLEPRE	SMWSTWTP-GLO-OLP-TF-RPT-000001	7.1 Road dilapidation report	D87	D87.c states the report must state the "details as to the date of completion of the road dilap surveys", this is not currently outlined in Section 7.1. Please include text detailing the date of completion.	Potential Non-Compliance	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	7.1 Road dilapidation report	D87	Refer to the below comment	Potential Non-Compliance	N
				22.01	2/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	7.1 Road dilapidation report	D87	Added such wording	Potential Non-Compliance	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	7.1 Road dilapidation report	D87	-	Potential Non-Compliance	N
				22.01.01	6/11/2023	SMD	SLEPRE				The condition requests that the date of completion of the road dilap surveys be mentioned, Section 7.1 of Rev B does not include this. Could you please include the date of which the surveys were completed	Potential Non-Compliance	N
					9/11/2023	GLO	DKELLY				Section 7.1 has been reworded to say "Road dilapidation reports has been provided for the local roads used by construction vehicles. These reports will be undertaken prior to the use of these roads. A copy of the report has been provided to the relevant road authority on 27th October 2023. The requirement is to submit this plan within three weeks of completing the survey and no later than one (1) month before the road is used. If damage to roads occurs as a result of heavy vehicle use associated with the construction works, GLC, will, at the relevant road authority's discretion: ●Compensate the relevant road authority for the damage so caused or ●Rectify the damage to restore the road to at least the condition it was in pre-work as identified in the road dilapidation report A copy of the Road Dilapidation Report transmittal to the Sydney Olympic Park Authority has been provided separately."	Potential Non-Compliance	N
				25	1/11/2023	SMD	SLEPRE	SMWSTWTP-GLO-OLP-TF-RPT-000001	7.1 Road dilapidation	D88	Suggest including attachment/appendix to show evidence that the road dilapidation has been provided to SOPA in accordance with D88. Refer to appendix in Section 7.1.	Potential Non-Compliance	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	7.1 Road dilapidation	D88	Refer to the below comment	Potential Non-Compliance	N

DOCUMENT NO.	TITLE	VER	STATUS	NO.	DATE	COMPANY	RAISED BY	REVIEW DOC. NO.*	DOCUMENT REF*	DEED REF*	COMMENTS / RESPONSE	COMMENT CATEGORY*	CLOSED OUT
				25.01	2/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	7.1 Road dilapidation	D88	Added	Potential Non-Compliance	N
					9/11/2023	GLO	DKELLY	SMWSTWTP-GLO-OLP-TF-RPT-000001	7.1 Road dilapidation	D88	This has been provided in appendix D.	Potential Non-Compliance	N
				25.01.01	6/11/2023	SMD	SLEPRE				Attachment only indicates that the road dilap reports were sent to Sydney Metro to pass on to SOPA, suggest including attachment to show evidence that these have since been provided to SOPA.	Potential Non-Compliance	N
					9/11/2023	GLO	DKELLY				Revised attachment.	Potential Non-Compliance	N

APPENDIX D – ROAD DILIPIDATION TRANSMITTAL

Sydney Metro West - WTP - Pre-Construction Condition Survey - Road Dilapidation Report - Sydney Olympic Park - Issued for Information

 Angela Lumsden via InEight Document <system@teambinder.com>
To: Kelly, Daniel


Wed 8/11/2023 2:42 PM

[If there are problems with how this message is displayed, click here to view it in a web browser.](#)

Document Transmittal

Transmittal No:	SMWSTWTP-GLO-TX-002315
Contract No:	WTP - 00013/13065 - Western Tunnelling Works Design and Construction Deed
Sub Contract:	
Date:	08 November 2023, 02:41 PM

Issued By	Name
Angela Lumsden	(Gamuda Laing O'Rourke Consortium)

Issued To	Name
Mark Farran	(Sydney Olympic Park Authority); Viveca Mcghee (Sydney Olympic Park Authority); Julie Currey (Sydney Olympic Park Authority); Sally Hamilton (Sydney Olympic Park Authority); Dylan Sargent (Sydney Olympic Park Authority); Grant Sutcliffe (Sydney Olympic Park Authority); Vivienne Albin (Sydney Olympic Park Authority); Rupert Luxton (Sydney Olympic Park Authority)
Cc:	Denniel Custodio (Sydney Metro); Daniel Kelly (Gamuda Laing O'Rourke Consortium); Demi Tascas (Sydney Metro); Patrick Austin (Sydney Metro); Nasim Sohrabi (Sydney Metro); Angela Lumsden (Gamuda Laing O'Rourke Consortium); Sam Besim (Gamuda Laing O'Rourke Consortium)

Reason for Issue	Issued for Information
Subject	Sydney Metro West - WTP - Pre-Construction Condition Survey - Road Dilapidation Report - Sydney Olympic Park - Issued for Information
<p>Good afternoon,</p> <p>Please find attached Sydney Metro West - WTP - Pre-Construction Condition Survey - Road Dilapidation Report - Sydney Olympic Park - Issued for Information</p> <p>Many thanks Angela Lumsden Document Controller Gamuda Australia and Laing O'Rourke Consortium Sydney Metro Western Tunnelling Package AJLevel 8, 60 Station Street, Parramatta, NSW 2150 Elangela.lumsden@glcwtw.com.au W www.gamuda.com.au W www.laingorourke.com</p>	

[Click here to download all Transmittal files.](#)

Item	Document No	Title	Rev	Sta	Page	Description & File Name	Alt. Description
1	SMWSTWTP-GLO-TX-002315-RPT-00001	Sydney Metro West - WTP - Pre-Construction Condition Survey - Road Dilapidation Report	A.01	S2	RPT		

APPENDIX E – STAKEHOLDER ENGAGEMENT PLAN

Communications Action Plan – GLC Truck Movements – Herb Elliot Avenue, Sydney Olympic Park

Once GLC commences truck movements at Sydney Olympic Park Sydney Metro site, the key stakeholders requiring consultation includes businesses adjacent to and along the site’s heavy vehicles route on Herb Elliot Avenue.

Communications Plan

Project phase/specific activity	Timing (indicative)	Proposed communications activity and purpose	Stakeholders identified	Tools	Status
Monthly construction update	1 November 2023	Construction update on construction activities, introduction of GLC’s scope of work and work area, traffic changes, hours of work and project contact details.	200m around the work area including businesses and hotels on Herb Elliot Avenue	Notification	Complete
Doorknock to introduce GLC and consult on truck movements	w/c 30 October or w/c 6 November	Carry out door knock to surrounding businesses for consultation regarding truck movements. AFJV to join in on doorknock for GLC introductions.	Pullman Hotel Ibis Hotel and Novotel 10 Herb Elliot (The Avenue) Abattoir Heritage Precinct (SOPA owned) 6 Herb Elliot	‘Sorry we missed you’ (SWMY) cards Project information cards	To-do

Doorknock to distribute December monthly construction update	w/c 27 November	Carry out door knock to surrounding businesses as a reminder about GLC commencing on site and distribute notification detailing December work activities. AFJV to join in on doorknock.	Pullman Hotel Ibis Hotel and Novotel 10 Herb Elliot (The Avenue) Abattoir Heritage Precinct (SOPA owned) 6 Herb Elliot Notification will go out to stakeholders 200m around the work area.	Notification	To-do
--	-----------------	--	---	--------------	-------

Feedback

Local businesses and stakeholders can reach out to the project via the information line: 1800 612 173, email: metrotunnelsGLC@transport.nsw.gov.au or via the mailing address: Sydney Metro West, PO BOX K659, Haymarket, NSW 1240. All these contact details are listed on the notifications and project information cards.

APPENDIX F – ROAD SAFETY AUDIT

Desktop Road Safety Audit

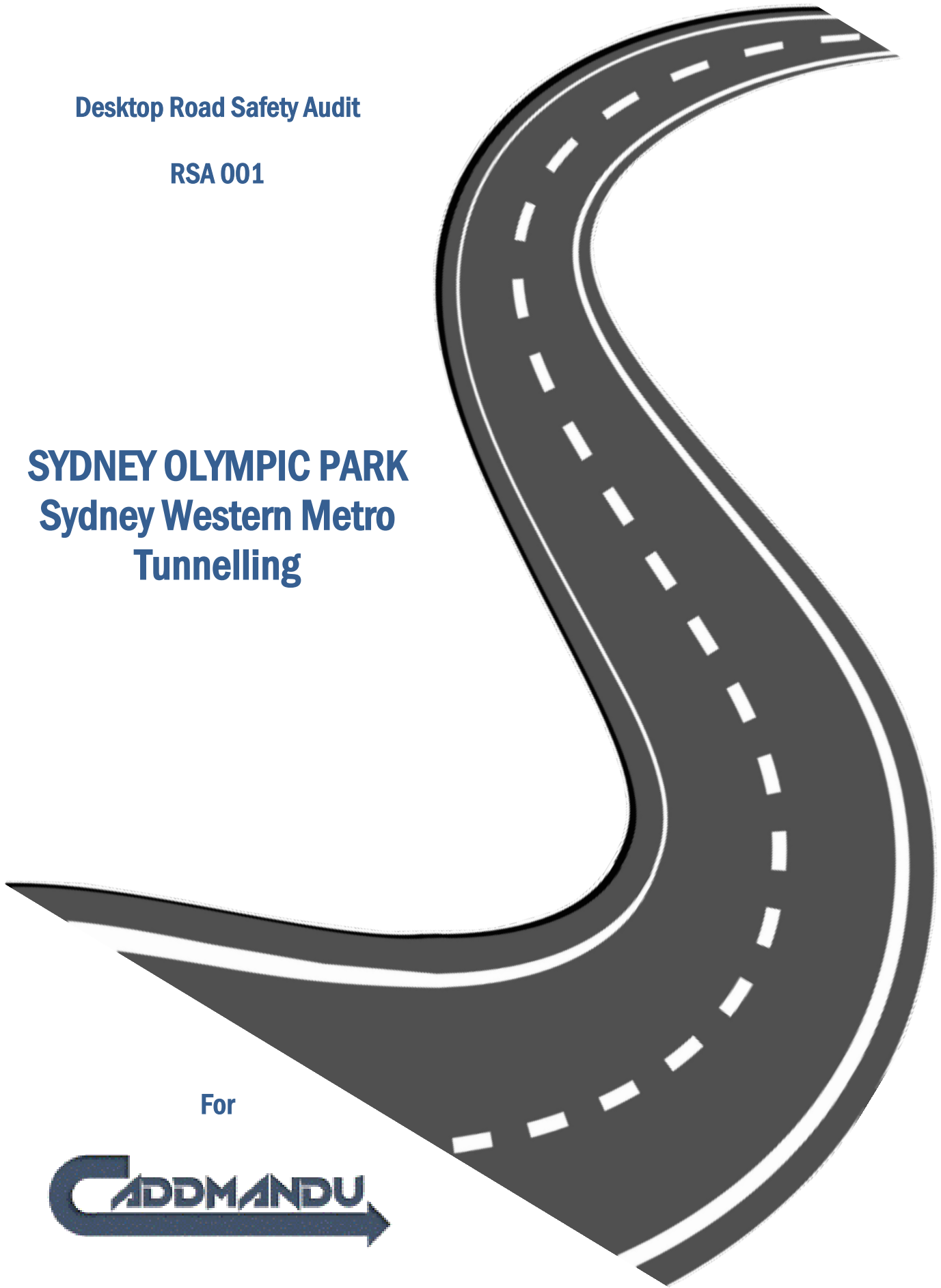
RSA 001

**SYDNEY OLYMPIC PARK
Sydney Western Metro
Tunnelling**





For



11 October 2023



Document Information Sheet

Edition / Revision No.	1	2	
Document Status	DRAFT (Internal Review)	Final	
Prepared By	Craig Nethery Director 	Craig Nethery Director 	
Reviewed By	Andy Davis Director 	Andy Davis Director 	
Date	11 October 2023	11 October 2023	
Issued To		Jason Cox, Director / Project Manager, Caddmandu Design & Drafting	

Disclaimer

This report contains findings and recommendations based on examination of the site and / or relevant documentation. The report is based on the conditions viewed on the day of inspection and is relevant at the time of production of the report. Information and data contained within this report is prepared with due care by the Road Safety Audit Team. While the Road Safety Audit Team seeks to ensure accuracy of the data, it cannot guarantee its accuracy.

Readers should not solely rely on the contents of this report or draw inferences to other sites. Users must seek appropriate expert advice in relation to their own particular circumstances.

The Road Safety Audit Team does not warrant, guarantee or represent that this report is free from errors or omissions or that the information is exhaustive. Information contained within may become inaccurate without notice and may be wholly or partly incomplete or incorrect. Before relying on the information in this report, users should carefully evaluate the accuracy, completeness and relevance of the data for their purposes.

Subject to any responsibilities implied in law which cannot be excluded, the Road Safety Audit Team is not liable to any party for any losses, expenses, damages, liabilities or claims whatsoever, whether direct, indirect or consequential, arising out of or referable to use of this report, however caused whether in contract, tort, statute or otherwise.

StreetWise Road Safety & traffic Services Pty Ltd
PO BOX 1395
Port Macquarie
NSW 2444

Mob:- 0412 009 558 (Craig Nethery)
Email:- craig@streetwisersa.com.au

Mob:- 0422 011 353 (Andy Davis)
Email:- andy@streetwisersa.com.au

www.streetwisersa.com.au



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1. INTRODUCTION

1.1 Scope of Audit

StreetWise Road Safety and Traffic Services has been engaged by the Caddmandu Design & Drafting to complete a Desktop Road Safety Audit of Traffic Guidance Schemes (formerly Traffic Control Plans) for Sydney Metro (light rail) construction works at the Sydney Olympic Park, Homebush, NSW.

This Road Safety Audit will be conducted in accordance with the Austroads Guide to Safety Part 6: Road Safety Audit (2022).

1.2 Scope Project Description

Sydney Metro is Australia's biggest public transport project, and by 2030, Sydney will have a network of four metro lines, 46 stations and 113km of new metro rail. Sydney Metro will connect Sydney's northwest, west, southwest and greater west to fast, reliable light rail services with fully accessible stations.

The metro program includes the operational Metro Northwest Line and three projects under construction:

- City & Southwest
- West
- Western Sydney Airport

There will be ultimate capacity for a metro train every two minutes in each direction under the city. Sydney's new metro railway will have a target capacity of about 40,000 customers per hour, similar to other metro systems worldwide. Sydney's current suburban system can reliably carry 24,000 people an hour per line.

Sydney Metro, together with signalling and infrastructure upgrades across the existing Sydney rail network, will increase the capacity of train services entering the Sydney CBD – from about 120 an hour today to up to 200 services beyond 2024.

The Western 24-kilometre metro line will double rail capacity between Greater Parramatta and the Sydney CBD, linking new communities to rail services and supporting employment growth and housing supply. Stations are confirmed at Westmead, Parramatta, Sydney Olympic Park, North Strathfield, Burwood North, Five Dock, The Bays, Pyrmont and Hunter Street in the Sydney CBD. Construction started in 2020, with the project on track to be completed by 2030.

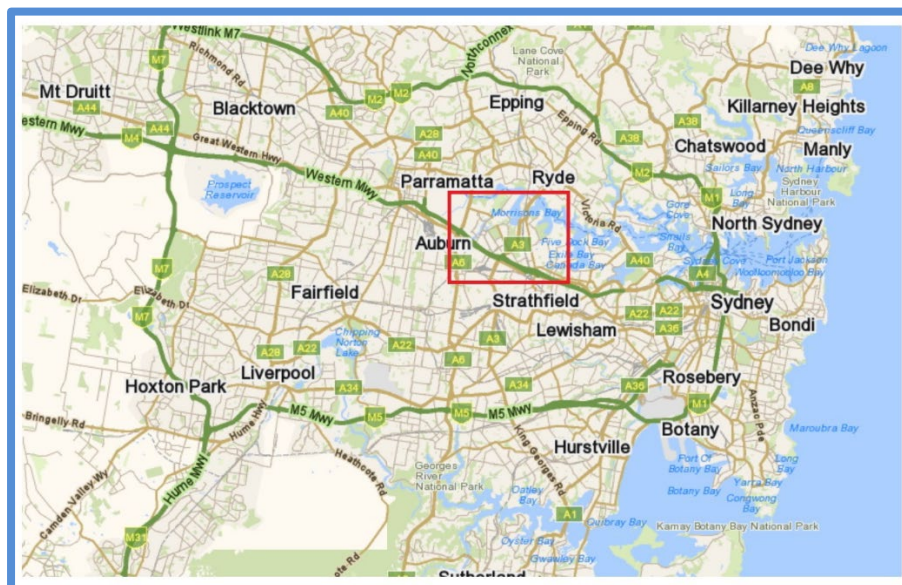


Figure 1: Locality Plan

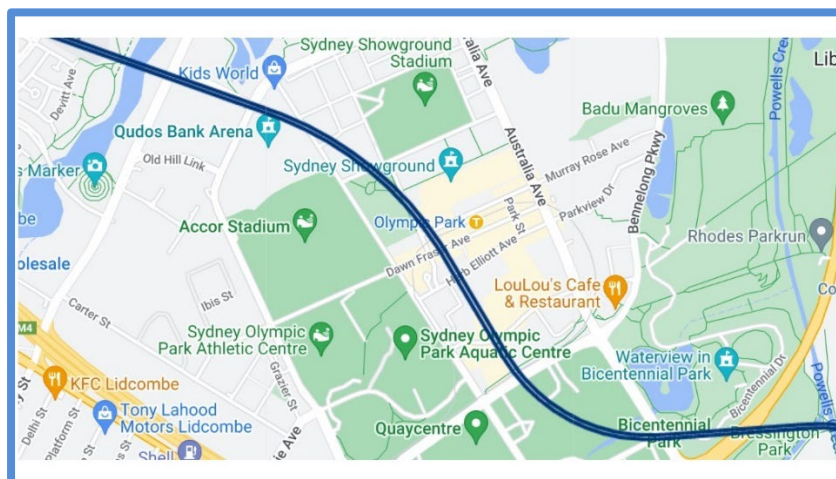


Figure 1: Site Plan

Background information on the project is contained within Section 4 of this report.

The audit was undertaken by a team lead by Andy Davis of StreetWise Road Safety & Traffic Services in compliance with the audit brief. (See Section 1.3 for Audit Team details)

StreetWise were provided Traffic Guidance Scheme plans and swept vehicle paths detailing the proposed traffic management of 19m semi-trailers movements in the vicinity of Herb Elliott Avenue at the Sydney Olympic site.

1.3 Reference Material

The design standards/manuals used to assess the proposal are as follows:

- Austroads 'Guide to Road Safety Part 6: Road Safety Audit (Jan 2022)'
- Austroads 'Guide to Temporary Traffic Management: Parts 1 to 10';
- Austroads 'Guide to Road Design';
- TfNSW Guidelines for Road Safety Audit Practices
- TfNSW Supplements to Austroads Guides
- TfNSW Traffic Control at Work Sites Manual
- Australian Standards

1.4 Audit Team

Auditor No. / Accreditation	Name	Role	Organisation
RSA-02-0230 Level 3 Auditor	Craig Nethery	Audit Team Member	StreetWise Road Safety & Traffic Services
RSA-02-0678 Level 3 Auditor	Andy Davis	Audit Team Leader	StreetWise Road Safety & Traffic Services

Table 1: List Of Audit Team Members

1.5 Stakeholders

The following people / organisations are listed as stakeholders in the completion of this Detailed Design Desktop Road Safety Audit.

Name	Role / Status	Job Title & Organisation
Jason Cox	Project Manager	Director Caddmandu Design & Drafting (Client)

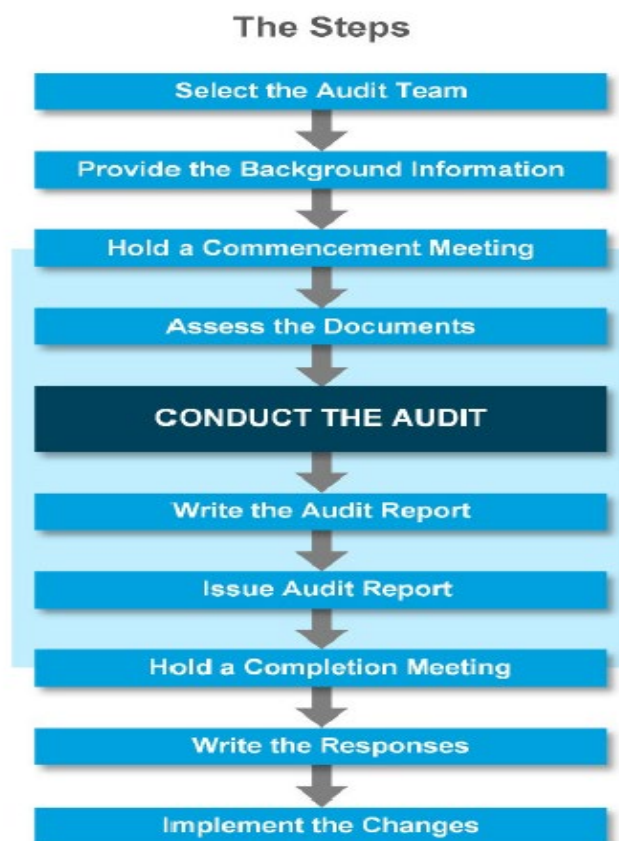
Table 2: List Of Stakeholders

1.6 Site Inspection

This is a desktop road safety audit. There was no requirement as part of the scope of the audit for a site inspection to be completed.

2. ROAD SAFETY AUDIT PROCESS

The following flow chart is reproduced from *Austrroads Guide to Road Safety – Part 6: Road Safety Audits*, Figure 8.1



A more detailed illustration of the process appears in Section 8.4.

Figure 2: Steps In A Road Safety Audit

3. SAFE SYSTEM AND RSA

The Safe System Assessment follows the principles outlined in *Austrroads Guide to Road Safety Part 6: Road Safety Audit (2022)*.

The aim of Safe System assessment is to identify any safety risks and hazards, including those not identified or addressed in any earlier, feasibility (strategic/concept) or preliminary design stage audit/s undertaken, and to ensure that the design considers all foreseeable road users.

The additional annotation “**IMPORTANT**” shall be used to provide emphasis to any road safety audit finding that has the potential to result in fatal or serious injury or findings that are likely to result in the following crash types above the related speed environment.

- Head- on (>70km/h)
- Right angle (>50km/h)
- Run off road impact object (>40km/h)
- Crashes involving vulnerable road users (>30km/h)

These crash types are known to result in higher severity outcomes at relatively lower speed environments. The exposure and likelihood of crash occurrence shall then be considered for all findings deemed “IMPORTANT” and evaluated based on an auditor’s professional judgement. Auditors should consider factors such as:-

- traffic volumes and movements
- speed environment
- crash history
- road environment

and apply road safety engineering and crash investigation experience to determine the likelihood of crash occurrence. The likelihood of crash occurrence shall be considered either:-

- VERY HIGH
- HIGH
- MODERATE
- LOW

This additional annotation shall be displayed following the “IMPORTANT” on applicable findings provided in Table 5 – Audit Findings.

3.1 Safe System Matrix Analysis

A safe system matrix analysis has been applied to this infrastructure to assess its conformance to the safe system principles as provided in Austroads Guide to Road Safety Part 6: Managing Road Safety Audits.

Project Safe System Matrix Analysis	
Project	Construction of the Sydney Metro (West) under the existing Olympic Site, Homebush.
Project Objective	Safely manage project-generated semi-trailer movements in & out of the project site.
Road Function	<p><u>Homebush Bay Drive</u> Existing major arterial road connecting the M5 and Parramatta Road in the south to Sydney’s northern suburbs, M2 motorway & Pacific Hwy in the north. Homebush Bay Rd also provides access to the Homebush Olympic precinct.</p> <p><u>Australia Avenue</u> Existing arterial road around the perimeter of the Homebush Olympic site, providing access from the local road network to various locations within the Olympic site.</p> <p><u>Herb Elliott Avenue</u> Existing internal road within the Olympic site.</p> <p><u>Olympic Boulevard</u> Existing internal road within the Olympic site.</p>
Speed Environment	<u>Homebush Bay Drive</u> 80kmh

	<p><u>Australia Avenue</u> 60kmh</p> <p><u>Herb Elliott Avenue & Olympic Boulevard</u> 40kmh</p>
Road Users / Facilities / Vehicle Composition	<p><u>Homebush Bay Drive</u> Homebush Bay Drive is a high quality sealed road in good condition. Caters for high volumes of light vehicles through to heavy vehicles (B Doubles).</p> <p><u>Australia Avenue</u> Australia Avenue is a high quality sealed road in good condition. Caters for high volumes of light vehicles through to heavy vehicles (B Doubles).</p> <p><u>Herb Elliott Avenue</u> Existing low speed environment that generally caters for low volumes. During events, the volumes of light vehicles and delivery vehicles are often high, with the road reserve also catering for high volumes of pedestrians.</p> <p><u>Olympic Boulevard</u> As above.</p>

Table 3: Safe System Matrix Summary

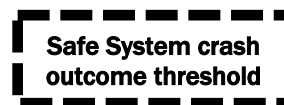
3.2 Road Safety Criteria

The list of road safety issues contained in Table 5 – Audit Findings, of the report contains rankings of safety issues which are based on Criteria set out in the Austroads *Guide to Road Safety Part 6: Road Safety Audit, (Feb 2022)*. The assessment of risk uses these principles. The tables from Austroads are reproduced below:

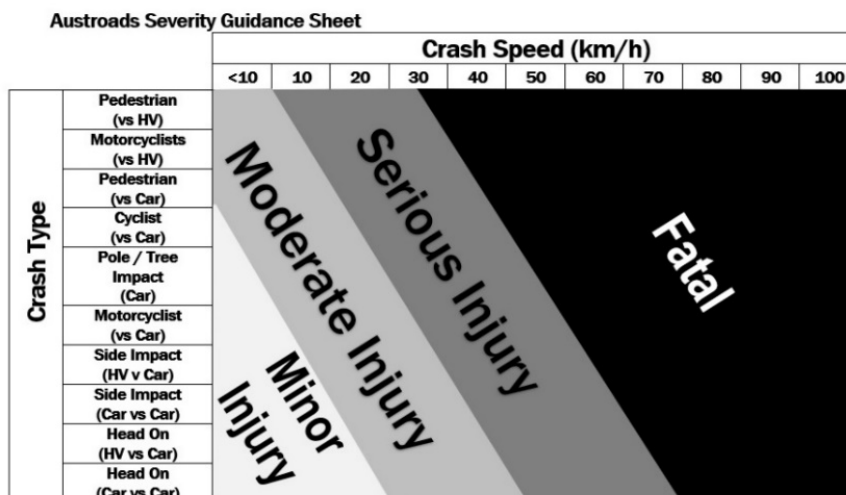
Austrroads RSA Risk Matrix

			Severity*				
			Insignificant	Minor	Moderate	Serious	Fatal
			Property Damage	Minor First Aid	Major First Aid and/or presents to Hospital	Admitted to Hospital	Death within 30 days of crash
Likelihood (includes exposure)	Almost Certain	One per Quarter	Medium	High	High	Extreme (FSI)	Extreme (FSI)
	Likely	Quarter to 1 Year	Medium	Medium	High	Extreme (FSI)	Extreme (FSI)
	Possible	1 to 3 Years	Low	Medium	High	High (FSI)	Extreme (FSI)
	Unlikely	3 to 7 Years	Negligible	Low	Medium	High (FSI)	Extreme (FSI)
	Rare	7 Years Plus	Negligible	Negligible	Low	Medium	High (FSI)

*See Severity Guidance Sheet



Austrroads Severity Guidance Sheet



Note
It is stressed that the information contained within the severity guidance sheet is a general indication only and that professional engineering judgement is required with its usage.

Note
It is stressed that the information contained within the severity guidance sheet is a general indication only and that professional engineering judgement is required with its usage.

Austrroads Priorities for Mitigation

Risk	Suggested Action
Negligible	No action required.
Low	Should be corrected or the risk reduced, if the treatment cost is low.
Medium	Should be corrected or the risk significantly reduced, if the treatment cost is moderate, but not high.
High	Should be corrected or the risk significantly reduced, even if the treatment cost is high.
Extreme	Must be corrected regardless of cost.

3.3 Previous Audits

Streetwise Road Safety and Traffic Services are not aware of any previous road safety audits for the works or location.

3.4 Identified Road Safety Issues

A summary of the audit findings are provided in Table 5 of this report.

Every effort has been made to identify potential safety hazards in this audit, no guarantee can be made that every issue has been identified. This will be the case with any road safety audit.

3.5 Responding to the Audit Report

As set out in the road safety audit guidelines, the responsibility for planning, road design and construction of the project always remains with the client and the implementation team, and not with the auditor/s. It is **not** the role of the auditor to redesign or take over construction implementation of projects but to provide

independent advice via audit findings and / or recommendations where requested by the client.

The client / implementation team are under no obligation to accept all the audit findings and or recommendations. Also, it is not the role of the auditor to agree to or approve of the client / implementation team's response to the audit. Rather, the audit provides the opportunity to highlight potential problems and have them formally considered by the client / implementation team, in conjunction with all other project considerations.

This formal road safety audit report should be responded to in writing following a completion meeting where the findings are explained to the client / implementation team. This response should indicate acceptance or rejection of the suggested remedial measures. Reasons are usually provided where a road safety deficiency or suggested remedial measure is rejected.

4. BACKGROUND DATA

The following data information was used as part of the assessment process in this audit:

- Design Plans,
- Traffic Data.

4.1 Design Plans & Documentation

A summary of the plan information and documentation used in this assessment is provided below:

Document No.	Rev	Title	Issue	Date
LGP-61447-GLC Sheets 1 - 4		Herb Elliott Avenue Sydney Oly Pk – Semi delivery	Approved	5/10/23
LGP-61447-GLC Sheets 1 - 14		Herb Elliott Avenue Sydney Oly Pk – Barrier Removal	Approved	5/10/23
LGP-61447-GLC Sheets 1 - 4		Herb Elliott Avenue Sydney Oly Pk – Permanent Signage Plan	Approved	5/10/23
D0089-DRG-TS-02 Sheets 1 - 16		Swept Paths (1)	AFC	2/10/23

4.2 Traffic Data

A check of the TfNSW traffic volume viewer website indicated that over 95,000 vehicles per day use Homebush Bay Road, with 7% heavy vehicle content.

Volumes on the internal roads within the Olympic site are generally low, apart from days when major events are held at the site.

5. TABLE 5 – AUDIT FINDINGS

Audit Results		Risk Level	Recommendation/s	Client Response	
Audit Finding Ref.	Audit Finding (Risk / Hazard, extent, crash type)			Accept (Yes / No)	Action / comments
Appendix B – TGS Herb Elliott Ave Plans					
LGP-61447-GLC _ Herb Elliott Avenue, Sydney Oly Pk – Semi delivery (Sheets 1 – 4)					
Sheets 1 - 4	Nil comments				
Appendix B – TGS Plans					
LGP-61447-GLC Herb Elliott Avenue, Sydney Oly Pk – Barrier Removal (Sheets 1 – 14)					
Sheet 4/14	<ul style="list-style-type: none"> Roadworks speed zone signage is required to be placed on each edge of the through traffic formation. (Table 4.9 – TCAWS V6.1) Suggest 40km/h Ahead signage given Homebush Bay Drive is a main road. All signage should be duplicated on RHS of through formation where multiple lanes. 		Comment Only		
Sheet 5/14	<ul style="list-style-type: none"> All signage should be duplicated on RHS of through formation where multiple lanes. 		Comment Only		
Sheet 6/14	<ul style="list-style-type: none"> Roadworks speed zone signage is required to be placed on each edge of the through traffic formation. (Table 4.9 – TCAWS V6.1) How will left slip onto Western Motorway On Ramp work when lanes on Homebush Bay Drive are reduced to 1 one lane and the 		Comment Only Comment Only		

5. TABLE 5 – AUDIT FINDINGS

Audit Results		Risk Level	Recommendation/s	Client Response	
Audit Finding Ref.	Audit Finding (Risk / Hazard, extent, crash type)			Accept (Yes / No)	Action / comments
	<p>distance between the end of the lane drop and the left slip turn is only 40m. How will the merge out of the Local Business Access work in peak traffic periods when Homebush Bay Drive is down to 1 lane.</p> <ul style="list-style-type: none"> How will traffic be managed in the event of a traffic incident during this traffic management setup 		Comment Only		
Sheet 7/14	<ul style="list-style-type: none"> How will traffic be managed during peak periods where there is a reduction in available lane thus reducing capacity and increasing traffic flow times. (i.e. Centenary Drive Off Ramp E/B) Will this impact traffic flow on the Western Motorway? 		Comment Only		
Sheet 8/14	<ul style="list-style-type: none"> Roadworks speed zone signage is required to be placed on each edge of the through traffic formation. (Table 4.9 – TCAWS V6.1) All signage should be duplicated on RHS of through formation where multiple lanes. 		Comment Only		
Sheet 9/14	<ul style="list-style-type: none"> All signage should be duplicated on RHS of through formation where multiple lanes. 		Comment Only		

5. TABLE 5 – AUDIT FINDINGS

Audit Results <i>Audit Finding Ref.</i>	Audit Finding (Risk / Hazard, extent, crash type)	Risk Level	Recommendation/s	Client Response	
				Accept (Yes / No)	Action / comments
Sheet 10/14	<ul style="list-style-type: none"> Roadworks speed zone signage is required to be placed on each edge of the through traffic formation. (Table 4.9 – TCAWS V6.1) All signage should be duplicated on RHS of through formation where multiple lanes. 		Comment Only		
Sheet 11/14	<ul style="list-style-type: none"> All signage should be duplicated on RHS of through formation where multiple lanes. 		Comment Only		
Sheet 12/14	Nil observations				
Sheet 13/14	Nil observations				
Sheet 14/14	Nil observations				
General	Will lane closures impact on other road users i.e. pedestrians, cyclists, garbage collection etc.		Comment Only		

LGP-61447-GLC Herb Elliott Avenue, Sydney Oly Pk – Permanent Signage Plan

5. TABLE 5 – AUDIT FINDINGS

Audit Results	Audit Finding (Risk / Hazard, extent, crash type)	Risk Level	Recommendation/s	Client Response	
Audit Finding Ref.				Accept (Yes / No)	Action / comments
Sheet 4/14	Nil observations				
D0089-DRG-TS-02 Swept Turnpaths (1) - (Sheets 1 -16)					
1 - 16	Will cars parked parallel to kerb reduce turnpath space i.e. will kerbside parking need to be closed off with signage/cones to ensure full road-width is available.		Comment Only		

6. CONCLUDING STATEMENT

The audit team certify as identified in this report it has examined the documentation provided and have inspected the site in undertaking this RSA. The audit team also confirm that this audit has been carried out in accordance with the *Austrroads Guide to Road Safety, Part 6 -Road Safety Audit (2022)* and in accordance with the Transport for NSW Works Authorisation Deed requirements.

The audit has been completed for the sole purpose of identifying any risks found within the design which could be mitigated to improve the road safety of the project.

The accompanying risks and associated recommendations and mitigation measures have been recorded for consideration by the Client for implementation.

- a) Prior to construction to improve the safety of the scheme. (Design Desktop Audits)
- ~~b) To improve the safety of the implemented constructed works / traffic scheme, (Pre or Post Opening / Traffic Scheme Audits) or~~
- ~~c) Identify any road safety issues that may be present as part of an existing traffic scheme. (Existing Conditions / Traffic Scheme Audits)~~
(delete inapplicable statement/s above)



Craig Nethery, Lead Road Safety Auditor
StreetWise Road Safety & Traffic Services Pty Ltd
Level 3 #RSA-02-0230
(Email:- craig@streetwisersa.com.au)



Andy Davis, Lead Road Safety Auditor
StreetWise Road Safety & Traffic Services Pty Ltd
Level 3 # RSA-02-0678
(Email:- andy@streetwisersa.com.au)

APPENDIX G – SOPA MEETING MINUTES

Meeting Minutes

WTP – GLC – SOPA Operations Minutes

Date:	18 th October 2023	Times:	Informal
Venue:	Microsoft Teams / email / phone calls		
Chairperson:	Nick Frost	Minutes:	Nick Frost
Attendees:	<p>(meeting not held just informal, tracking information)</p> <p>Nick Frost – GLC Interface Manager</p> <p>Jason Jung – GLC - Interface Manager – Interface Contractors</p> <p>Viveca Mcghie - Infrastructure and Environmental Compliance Officer Sydney Olympic Park Authority</p> <p>Kate Brooks – Sydney Metro - Interface</p> <p>Daniel Kelly – GLC Logistic Project Manager</p> <p>Candice Somerville – GLC - Environment Approval Manager</p> <p>Tahli Moore – GLC - Senior Environmental Advisor</p> <p>Charlotte Barton – GLC – Communications Advisor</p>		
Apologies:			

	Agenda item	Responsibility	Date
1	Presentation held with GLC / Sydney Metro / SOPA Operations 12/10/23 - No issues or concerns	NF/DK	12/10/23
Proposed Scope requests to SOPA			
2	<u>Survey I & M monitoring points installation</u> <ul style="list-style-type: none"> Required prior to Xmas 2023 Email sent to Viv 17/10/23 - waiting on response regarding process and work permits Site walks required? 	VM/NF	18/10/23
3	<u>Parking requests for site parking Secure parking stations</u> <ul style="list-style-type: none"> As per presentation held 12/10/23, SOPA has confirmed via email 16/10/23. P3 and P4 are available to use. GLC to respond and confirm 18/10/23 	GLC/SOPA	18/10/23
4	<u>Monitoring well – samples taken</u> (low impact) <ul style="list-style-type: none"> Ongoing 	GLC	Note
5	<u>Drone flights every 2 weeks</u> <ul style="list-style-type: none"> Not confirmed if required 	GLC	Note
6	<u>Truck, trailer and plant movements</u> <ul style="list-style-type: none"> Haulage routes Dilapidation reports - completed Possible vegetation trimming required (point cloud still under review) 	GLC	18/10/23
Current Scopes and possible impacts to SOPA			
7	<u>Dilapidation reports</u> <ol style="list-style-type: none"> Haulage routes – completed Herb Elliott Ave – to be completed with GLC move to site <ol style="list-style-type: none"> SOPA to confirm what is required in the frontage dilapidation report 	GLC/SOPA	ongoing

	Agenda item	Responsibility	Date
Future Scope			
8	<u>Herb Elliott Ave - Dilapidation</u> report – Mid Nov or Early Dec <ul style="list-style-type: none"> SOPA to confirm scope 	SOPA/GLC	18/10/23
9	<u>I & M survey Installation</u> of the following: as per presentation <ol style="list-style-type: none"> Ground Settlement points Single Bench Mark – hand auger 2m Building monitoring points 	GLC	18/10/23
10	<u>Trim trees along Haulage route</u> <ul style="list-style-type: none"> What's the SOPA process? <ul style="list-style-type: none"> Permit to trim trees Heritage Permit to work Any other requirements? GLC needs to review and complete all approvals. TBM movement not till April/May 2024 but need to run all approvals in parallel to ensure site can complete the works 	SOPA/GLC	18/10/23
11	<u>Establish site.</u> (date TBC mid Nov or Early Dec) <ul style="list-style-type: none"> Install site sheds Safety and environmental controls Scope works within site (minor works) no bulk excavation Use of haulage routes Install noise and vibration monitors etc 	GLC	18/10/23
Other Business			
12	<u>Upcoming SOPA Events:</u> available via the SOP website: <ul style="list-style-type: none"> SOPA's What's on Calendar flags upcoming events longer term (although please note this is a living calendar that is updated as required) https://www.sydneyolympicpark.com.au/whats-on SOPA's interactive map where you can click on the road closure tab to highlight upcoming road closures (although once again please note this is subject to updates as required) https://www.sydneyolympicpark.com.au/map/ 	SOPA	
13	<u>Metro Community number</u> (if required) 1800 612 173	Metro/GLC	Note
14	<u>Questions, Concerns from SOPA</u> <ul style="list-style-type: none"> 	SOPA	18/10/23
15	<u>SOPA Comms monthly meeting</u> with AFJV <ul style="list-style-type: none"> held monthly 25th Oct 2023 invite send to NF 	SOPA/GLC	18/10/23
16	<u>SOPA Construction monthly meeting held with AFJV</u> <ul style="list-style-type: none"> review and note if required NF to review with site team and AFJV 	SOPA/GLC	Note
17	Meetings held AHOC coordinated between NF and VM	NF/VM	Note
	Close	Chair	

Meeting Minutes

WTP – GLC – SOPA Operations Minutes

Date:	31 st October 2023	Times:	Informal
Venue:	Microsoft Teams / email / phone calls		
Chairperson:	Nick Frost	Minutes:	Nick Frost
Attendees:	<p>(meeting not held just informal, tracking information)</p> <p>Nick Frost – GLC Interface Manager</p> <p>Jason Jung – GLC - Interface Manager – Interface Contractors</p> <p>Viveca Mcghie - Infrastructure and Environmental Compliance Officer Sydney Olympic Park Authority</p> <p>Kate Brooks – Sydney Metro - Interface</p> <p>Daniel Kelly – GLC Logistic Project Manager</p> <p>Candice Somerville – GLC - Environment Approval Manager</p> <p>Tahli Moore – GLC - Senior Environmental Advisor</p> <p>Charlotte Barton – GLC – Communications Advisor</p>		
Apologies:			

	Agenda item	Responsibility	Date
1	Presentation held with GLC / Sydney Metro / SOPA Operations 12/10/23 - No issues or concerns	NF/DK	12/10/23
Proposed Scope requests to SOPA			
2	<u>Survey I & M monitoring points installation</u> <ul style="list-style-type: none"> Required prior to Xmas 2023 Email sent to Viv 17/10/23 - waiting on response regarding process and work permits VM provided response email re WP process on Mon 23rd October (11:51am) Site walks required? VM under the impression that good location maps associated with the WP will mean a site walk isn't required – however, happy to organise if down the track this is what best suits. 	VM/NF	31/10/23
3	<u>Parking requests for site parking Secure parking stations</u> <ul style="list-style-type: none"> As per presentation held 12/10/23, SOPA has confirmed via email 16/10/23. P3 and P4 are available to use. GLC to respond and confirm 18/10/23 No comments 	GLC/SOPA	31/10/23
4	<u>Monitoring well – samples taken</u> (low impact) <ul style="list-style-type: none"> Ongoing 	GLC	Note
5	<u>Drone flights</u> every 2 weeks <ul style="list-style-type: none"> Not confirmed if required 	GLC	Note
6	<u>Truck, trailer and plant movements</u> <ul style="list-style-type: none"> Haulage routes Dilapidation reports – completed VM to receive dilap reports from GLC once completed <ul style="list-style-type: none"> Haulage Frontage 	GLC	31/10/23

	Agenda item	Responsibility	Date
	<ul style="list-style-type: none"> • Vegetation trimming required (point cloud still under review) <ul style="list-style-type: none"> ◦ VM send permit and possible use of other route to not damage certain trees Email (31/10/23) 		
Current Scopes and possible impacts to SOPA			
7	<u>Dilapidation reports</u> <ol style="list-style-type: none"> 1. Haulage routes – completed 2. Herb Elliott Ave – to be completed with GLC move to site <ol style="list-style-type: none"> a. SOPA to confirm what is required in the frontage dilapidation report 	GLC/SOPA	ongoing
Future Scope			
8	<u>Herb Elliott Ave - Dilapidation</u> report – Mid Nov or Early Dec <ul style="list-style-type: none"> • SOPA to confirm scope • VM provided internal SOPA comment via email on scope of dilap for Herb Elliot on Mon 23rd October (08:58am) 	SOPA/GLC	31/10/23
9	<u>I & M survey Installation</u> of the following: as per presentation <ol style="list-style-type: none"> 1. Ground Settlement points 2. Single Bench Mark – hand auger 2m 3. Building monitoring points <ol style="list-style-type: none"> a. GLC to complete permit and submit (with Rob Roles – GLC) 	GLC	31/10/23
10	<u>Trim trees along Haulage route</u> <ul style="list-style-type: none"> • What's the SOPA process? <ul style="list-style-type: none"> ◦ Permit to trim trees ◦ Heritage ◦ Permit to work • Any other requirements? • GLC needs to review and complete all approvals. TBM movement not till April/May 2024 but need to run all approvals in parallel to ensure site can complete the works • VM provided email re Tree trimming on Mon 23rd October (12:01am), and further follow up email Tuesday 31st October 	SOPA/GLC	31/10/23
11	<u>Establish site.</u> (date TBC mid Nov or Early Dec) <ul style="list-style-type: none"> • Install site sheds • Safety and environmental controls • Scope works within site (minor works) no bulk excavation • Use of haulage routes • Install noise and vibration monitors etc 	GLC	18/10/23
Other Business			
12	<u>Upcoming SOPA Events:</u> available via the SOP website: <ul style="list-style-type: none"> - SOPA's What's on Calendar flags upcoming events longer term (although please note this is a living calendar that is updated as required) https://www.sydneyolympicpark.com.au/whats-on - SOPA's interactive map where you can click on the road closure tab to highlight upcoming road closures (although once again please not this is subject to updates as required) https://www.sydneyolympicpark.com.au/map/ 	SOPA	
13	<u>Metro Community number</u> (if required) 1800 612 173	Metro/GLC	Note

	Agenda item	Responsibility	Date
14	<u>Questions, Concerns from SOPA</u> <ul style="list-style-type: none"> • <u>none</u> 	SOPA	31/10/23
15	<u>SOPA Comms monthly meeting with AFJV</u> <ul style="list-style-type: none"> • held monthly 25th Oct 2023 – Completed • November monthly scheduled for 29th October 2023 	SOPA/GLC	31/10/23
16	<u>SOPA Construction monthly meeting held with AFJV</u> <ul style="list-style-type: none"> • review and note if required NF to review with site team and AFJV • VM still contemplating the best way forward with this – either GLC join AFJV's SOPA Construction monthly meeting, or GLC and SOPA meet separate to AFJV for GLC Construction monthly meeting. VM open to NF's thoughts on this. 	SOPA/GLC	31/10/23
17	Meetings held AHOC coordinated between NF and VM	NF/VM	Note
	<u>Close</u>	Chair	